



**COUNCIL WORK SESSION**  
**Tuesday, May 24, 2016**  
**6:30 p.m.**  
**Coon Rapids City Center**  
**Conference Room 1**

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**Call to Order**

Pursuant to Minnesota Statute 13D.04, subd. 2, the City Council will meet in work session to discuss the following:

1. PC 16-9 Amendment to Gateway Commerce Center PUD
2. Consider Phase One Boulevard Park Layout
3. 2018 Comprehensive Plan Update

**Other Business**

**Adjourn**



## City Council Work Session

1.

**Meeting Date:** 05/24/2016

**Subject:** PC 16-9 Amendment to Gateway Commerce Center PUD

**From:** Scott Harlicker, Planner

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### **INTRODUCTION**

The developer has requested an amendment to the Gateway Commerce Center PUD agreement to allow an expanded new car dealership.

### **DISCUSSION**

At the May 17th meeting, some Councilmembers expressed concerns regarding the size of the proposed dealership and the number of amendments that the developer has requested. To date, the Council has considered and approved four amendments to the PUD. Below is a list of the approved amendments:

- December 18, 2007 PUD originally approved.
- July 9, 2009 First PUD amendment: Holiday Store approved, site was originally approved as a bank and convenience store was located in back. Uses were flipped so that the convenience store was located at the corner and the bank behind it.
- September 3, 2013 Second PUD amendment: McDonalds approved for the site originally shown as a drug store.
- December 2, 2014 Third PUD amendment: Taco Bell approved for the site originally shown as a bank.
- April 8, 2015 Fourth PUD amendment: Walser car dealership and swim center approved, the car dealership site was originally shown as a 30,000 sf retail/office use and the swim center site was a 50,000 sf retail/office use. The hotel and one restaurant site were also included.

Those amended site plans and the original approved plan are attached. As part of the original 2007 PUD, the City also approved PUD Design Guidelines, which are also attached. There have been no amendments to the design guidelines.

The applicant is proposing to incorporate the lot that was identified for a swim center into the car dealership making it a 10 acre site. On that site will be a 60,000 square foot building and space for 616 cars. Attached is the proposed site plan.

The developer will be in attendance at the meeting.

### **RECOMMENDATION**

Staff and the developer are requesting direction from Council on how best to proceed with the proposed amendment. Initially, the 60 day clock ran until May 13, 2016, but was extended an additional 60 days to July 12, 2016. The Council will need to take formal action on both the PUD and the 4th addition plat before that date.

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**Attachments**

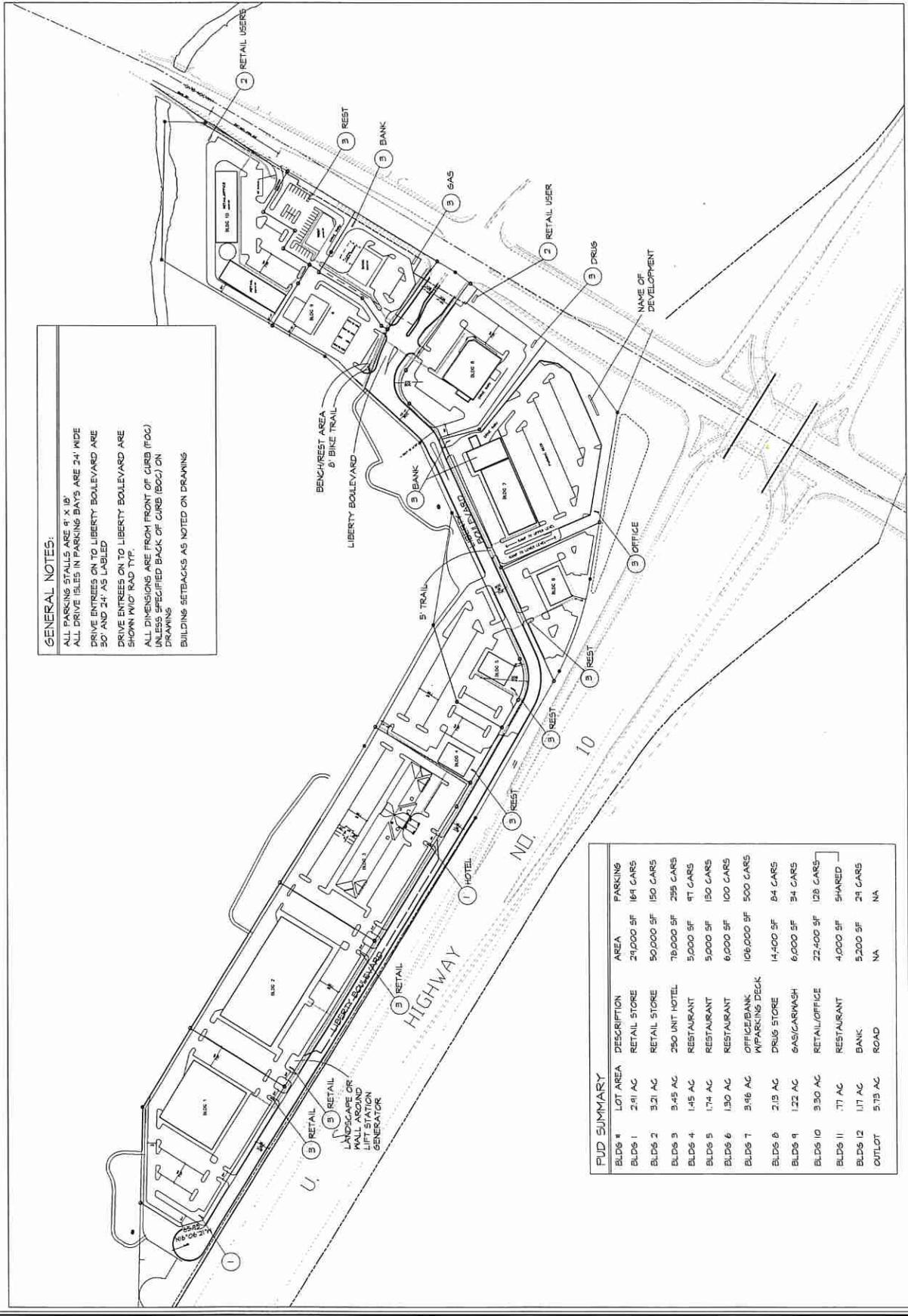
Original PUD Site Plan and Amendments

Proposed Amendment

PUD Design Guidelines

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**GENERAL NOTES:**  
ALL PARKING STALLS ARE 9' X 18'  
ALL DRIVE ISLES IN PARKING BAYS ARE 24' WIDE  
DRIVE ENTRIES ON TO LIBERTY BOULEVARD ARE  
30' AND 24' AS LABELED  
DRIVE ENTRIES ON TO LIBERTY BOULEVARD ARE  
SHOWN W/O RAD TYP.  
ALL DIMENSIONS ARE FROM FRONT OF CURB (FOC)  
(UNLESS SPECIFIED BACK OF CURB (BOC)) ON  
DRAWING  
BUILDING SETBACKS AS NOTED ON DRAWING

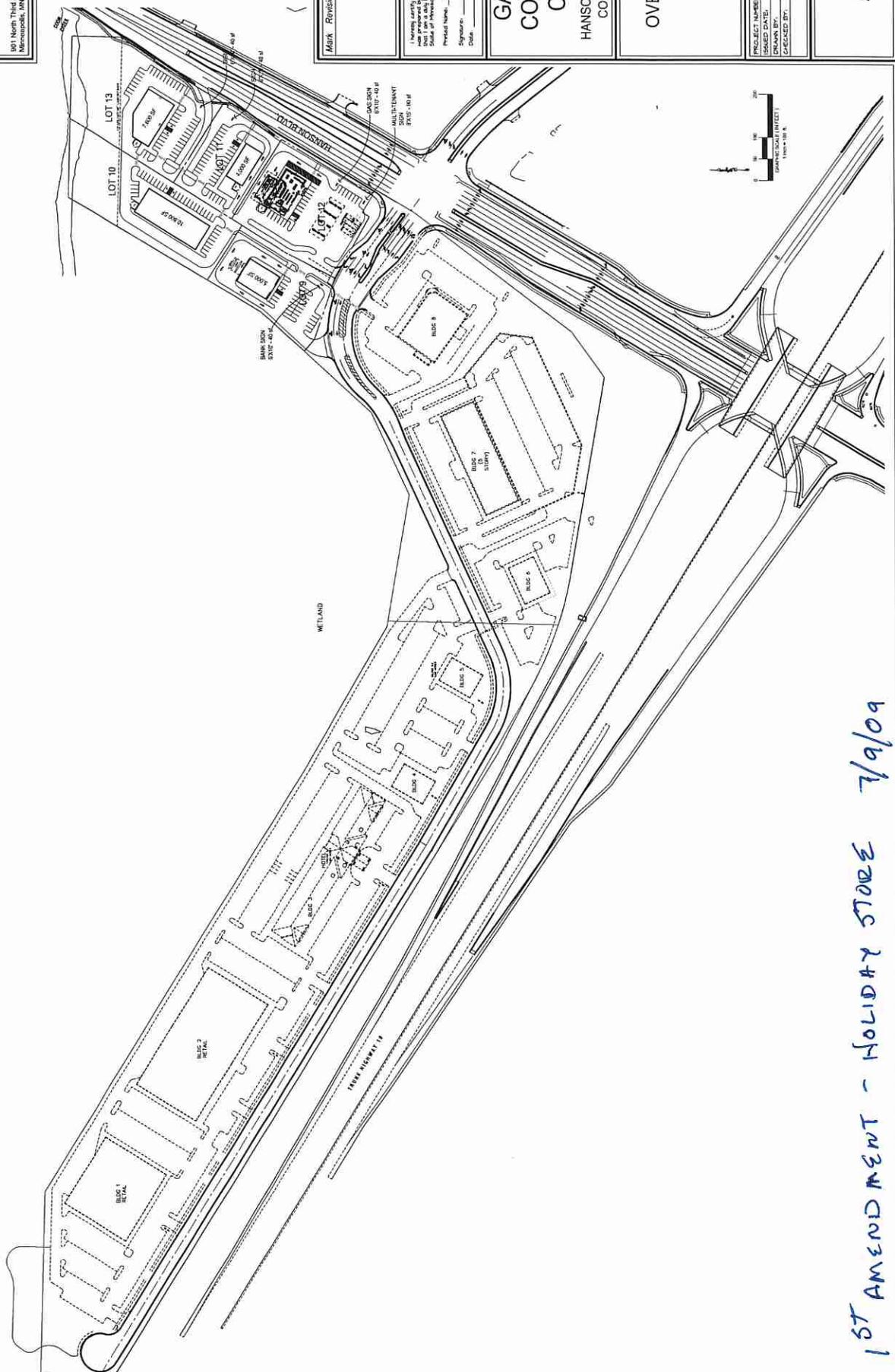


**PUD SUMMARY**

BLDG #	LOT AREA	DESCRIPTION	AREA	PARKING
BLDG 1	2.91 AC	RETAIL STORE	29,000 SF	184 CARS
BLDG 2	3.21 AC	RETAIL STORE	50,000 SF	150 CARS
BLDG 3	3.45 AC	250 UNIT HOTEL	78,000 SF	295 CARS
BLDG 4	1.45 AC	RESTAURANT	5,000 SF	41 CARS
BLDG 5	1.74 AC	RESTAURANT	5,000 SF	150 CARS
BLDG 6	1.30 AC	RESTAURANT	6,000 SF	100 CARS
BLDG 7	3.16 AC	OFFICE/BANK W/PARKING DECK	108,000 SF	500 CARS
BLDG 8	2.13 AC	DRUG STORE	14,400 SF	84 CARS
BLDG 9	1.22 AC	SAS/CARWASH	6,000 SF	34 CARS
BLDG 10	3.30 AC	RETAIL/OFFICE	22,400 SF	128 CARS
BLDG 11	.71 AC	RESTAURANT	4,000 SF	SHARED
BLDG 12	1.17 AC	BANK	5,200 SF	24 CARS
OUTLOT	5.19 AC	ROAD	NA	NA

ORIGINAL PLAN 12/18/07

**ARCHITECTURAL  
CONSORTIUM L.L.C.**  
101 North Third Street, Suite 220  
Minnetonka, MN 55311  
612-936-4030  
Fax: 612-932-5980



Mark	Revision / Issue	Date

I warrant, certify that the plan, specification or report does not contain any false information or misstatements of fact and that I am a duly Licensed Architect under the laws of the State of Minnesota.  
 Project Name: Gateway Commerce Center  
 License #: \_\_\_\_\_  
 Signature: \_\_\_\_\_  
 Date: \_\_\_\_\_

**GATEWAY  
COMMERCE  
CENTER**  
HANSON BLVD. & HWY. 10  
COON RAPIDS, MN

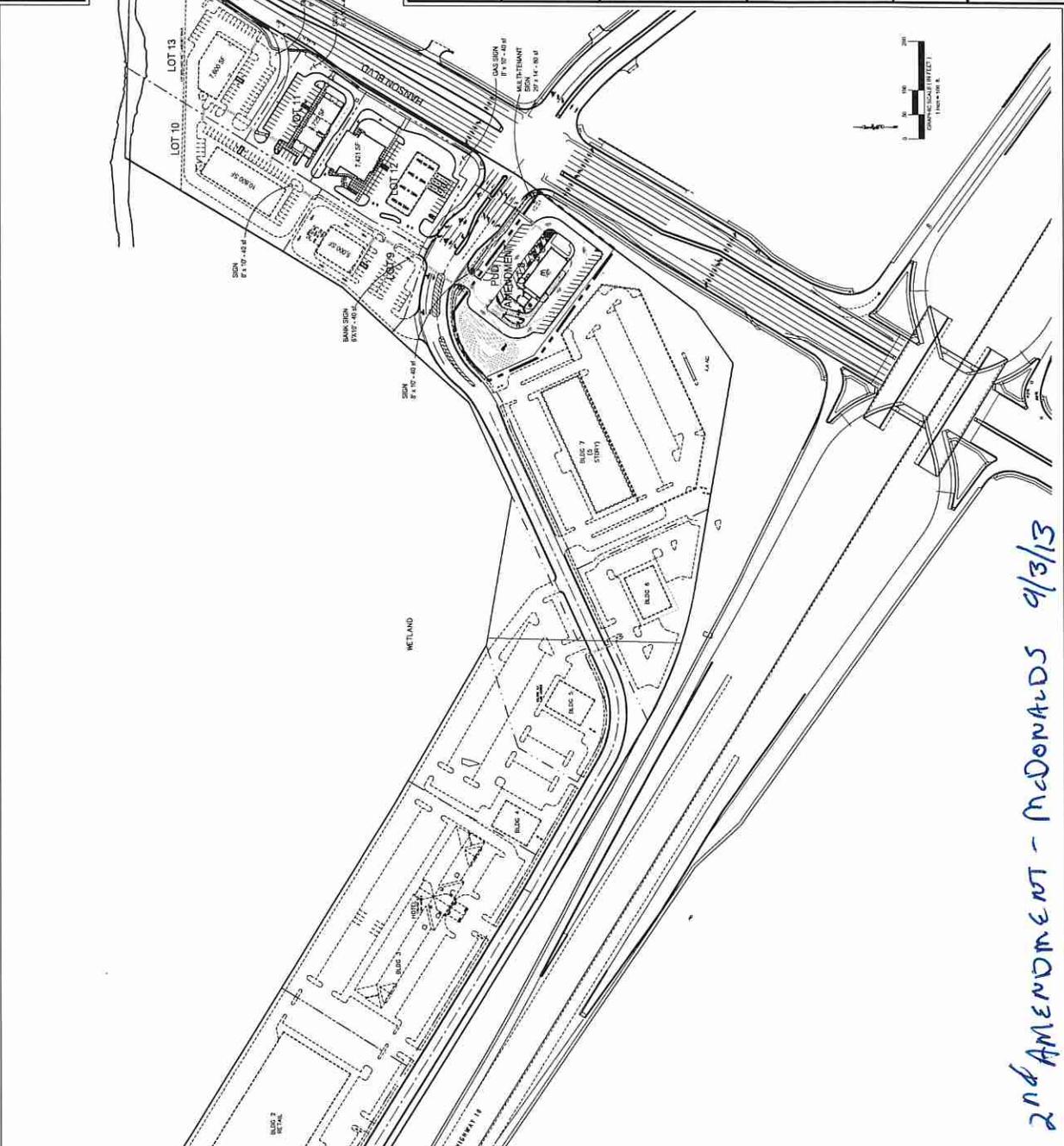
**OVERALL SITE  
PLAN**

PROJECT NUMBER: 04-010-C  
 ISSUED DATE: 02-2-04  
 DRAWN BY: DL  
 CHECKED BY: N.A.

**A1.1**

Architectural Consortium, L.L.C. 2009

*1st AMENDMENT - HOLIDAY STORE 7/9/09*



Mark	Revision / Issue	Date

**GATEWAY  
COMMERCE  
CENTER**  
HANSON BLVD. & HWY. 10  
COON RAPIDS, MN

**OVERALL SITE  
PLAN**

PROJECT NUMBER:	04-0710-0
ISSUED DATE:	04-04-13
DRAWN BY:	DL
CHECKED BY:	DL

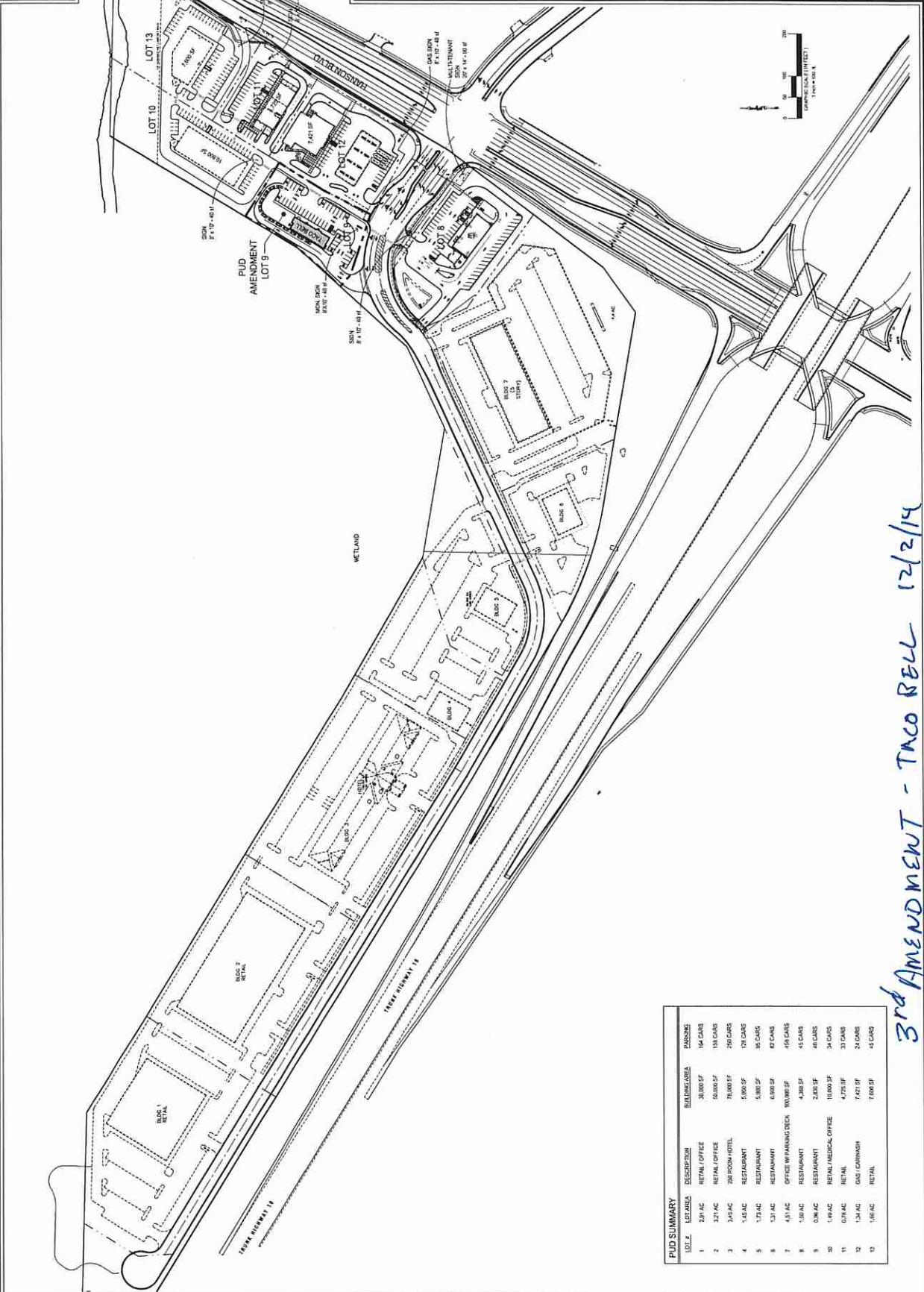
A1

LOT #	LOC AREA	DESCRIPTION	BUILDING AREA	PARKING
1	2.81 AC	RETAIL OFFICE	30,000 SF	114 CARS
2	3.21 AC	RETAIL OFFICE	50,000 SF	181 CARS
3	3.45 AC	208 ROOM HOTEL	78,000 SF	250 CARS
4	1.45 AC	RESTAURANT	5,000 SF	120 CARS
5	1.12 AC	RESTAURANT	5,000 SF	96 CARS
6	1.37 AC	RESTAURANT	6,000 SF	80 CARS
7	4.51 AC	OFFICE W/ PARKING DECK	18,000 SF	450 CARS
8	1.58 AC	RESTAURANT	4,200 SF	45 CARS
9	0.89 AC	BANK	5,000 SF	25 CARS
10	1.48 AC	RETAIL MEDICAL OFFICE	10,000 SF	34 CARS
11	0.78 AC	RETAIL	4,720 SF	31 CARS
12	1.34 AC	GAS LAUNDRY	7,471 SF	24 CARS
13	1.68 AC	RETAIL	7,800 SF	45 CARS

*2<sup>ND</sup> AMENDMENT - McDONALDS 9/3/13*

MARK	REVISION / ISSUE	DATE
<b>GATEWAY COMMERCE CENTER</b> HANSON BLVD. & HWY. 10 COON RAPIDS, MN		
<b>OVERALL SITE PLAN</b>		
PROJECT NUMBER	04-109-01	
ISSUED DATE	04-08-14	
DRAWN BY	BL/BD	
CHECKED BY	NA	

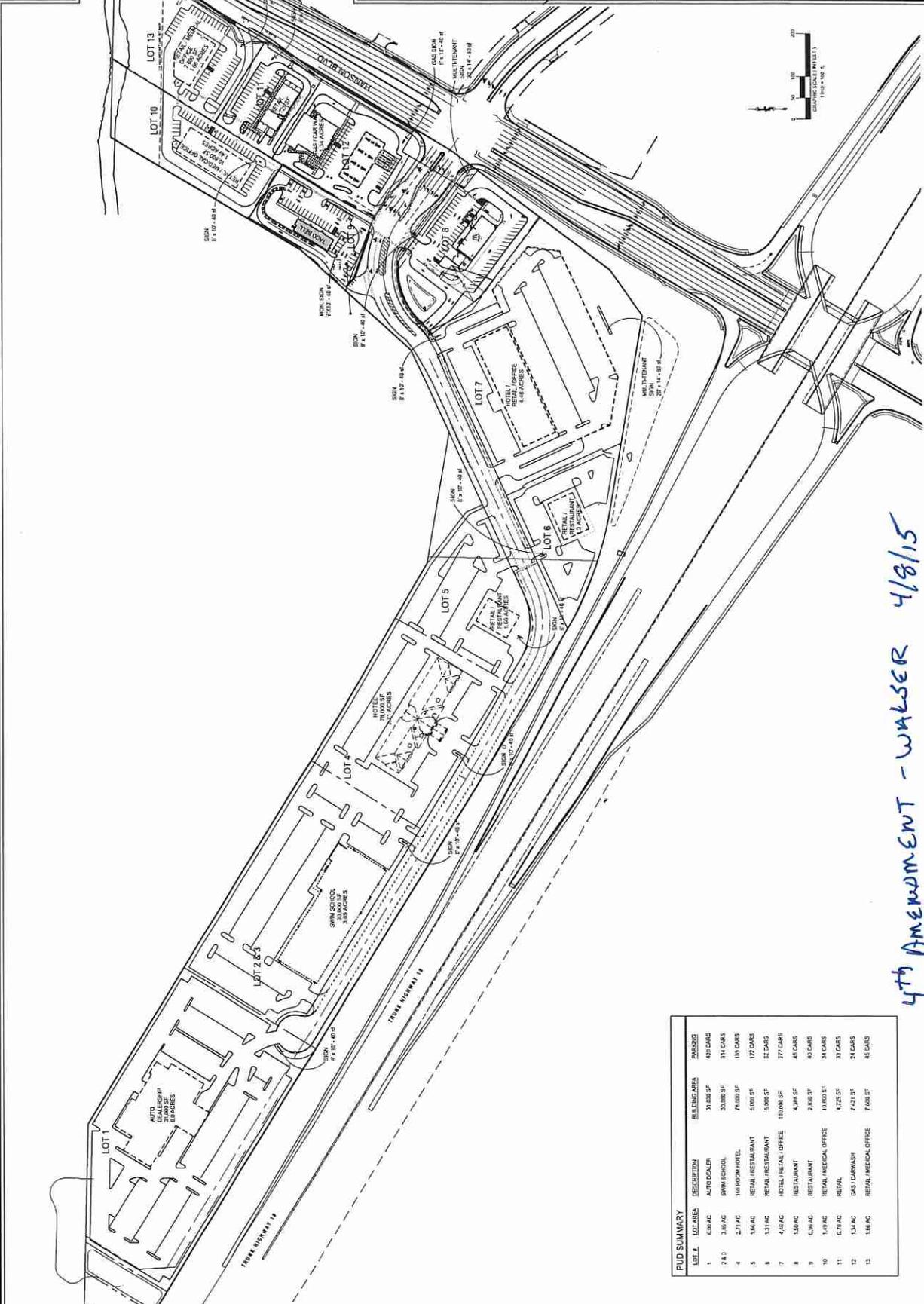
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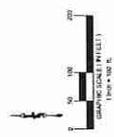
**PUD SUMMARY**

LOT #	ESTIMATED	DESCRIPTION	BUILDING AREA	PARKING
1	2.01 AC	RETAIL / OFFICE	30,000 SF	104 CARS
2	3.21 AC	RETAIL / OFFICE	60,000 SF	158 CARS
3	3.45 AC	200 ROOM HOTEL	70,000 SF	260 CARS
4	1.45 AC	RESTAURANT	5,000 SF	107 CARS
5	1.73 AC	RESTAURANT	5,000 SF	95 CARS
6	1.31 AC	RESTAURANT	6,000 SF	82 CARS
7	4.31 AC	OFFICE WITH PARKING DECK	100,000 SF	458 CARS
8	1.58 AC	RESTAURANT	4,300 SF	45 CARS
9	0.96 AC	RESTAURANT	2,800 SF	41 CARS
10	1.09 AC	RETAIL / MEDICAL OFFICE	10,000 SF	34 CARS
11	0.78 AC	RETAIL	4,175 SF	33 CARS
12	1.24 AC	DAY / CARWASH	7,421 SF	24 CARS
13	1.58 AC	RETAIL	7,000 SF	43 CARS

3rd AMENDMENT - TACO BELL 12/2/14



Mark	Revision / Issue	Date
<b>GATEWAY COMMERCE CENTER</b> HANSON BLVD. & HWY. 10 COON RAPIDS, MN		
<b>OVERALL SITE PLAN</b>		
PROJECT NUMBER:	CH-09-00	
DRAWN BY:	02-10-15	
CHECKED BY:	BL-00	
	K.A.	
A1.1		



**PLD SUMMARY**

LOT #	LOT AREA	DESCRIPTION	BUILDING AREA	PARKING
1	6.09 AC	AUTO DEALER	31,000 SF	430 CARS
2, 3	3.85 AC	SCHOOL	30,000 SF	314 CARS
4	2.71 AC	140 ROOM HOTEL	74,000 SF	181 CARS
5	1.65 AC	RETAIL-RESTAURANT	5,000 SF	122 CARS
6	1.31 AC	RETAIL-RESTAURANT	6,000 SF	82 CARS
7	4.68 AC	HOTEL/RETAIL/OFFICE	180,000 SF	277 CARS
8	1.50 AC	RESTAURANT	4,300 SF	45 CARS
9	0.98 AC	RESTAURANT	2,800 SF	40 CARS
10	1.49 AC	RETAIL/MEDICAL OFFICE	16,000 SF	34 CARS
11	0.78 AC	RETAIL	4,725 SF	33 CARS
12	1.30 AC	GAS/LAUNDRY	7,421 SF	24 CARS
13	1.68 AC	RETAIL/MEDICAL OFFICE	7,000 SF	45 CARS

4th AMENDMENT - WALSER 4/8/15

**Certification**

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed LANDSCAPE ARCHITECT under the laws of the State of Minnesota.

*William L. Delaney*

William L. Delaney  
Registration No. 40252 Date: 02/20/2015

This certification is not valid unless wet signed in blue ink. If applicable, contact us for a wet signed copy of this survey which is available upon request at Sambatek, Minnetonka, MN office.

**Summary**

Designed: bdb Drawn: JMT  
Approved: MCA Book / Page:  
Phase: PUD Initial Issue: 02/20/2015

**Revision History**

No.	Date	By	Submittal / Revision
A	10/26/07	ELF	REVISIONS PER CITY COMMENT
B	12/07/07	LAW	REVISIONS PER CITY COMMENT
C	05.07.09	kit	PUD REVISION
D	06.07.13	kit	PUD REVISION
E	08.05.15	kit	PUD REVISION FOR McDONALDS
G	02/20/15	WLD	PUD REVISION
H	03.14.16		PUD REVISION
I	05.10.16		PUD REVISION

**Sheet Title**  
**LANDSCAPE PLAN**  
**- SECTION A**

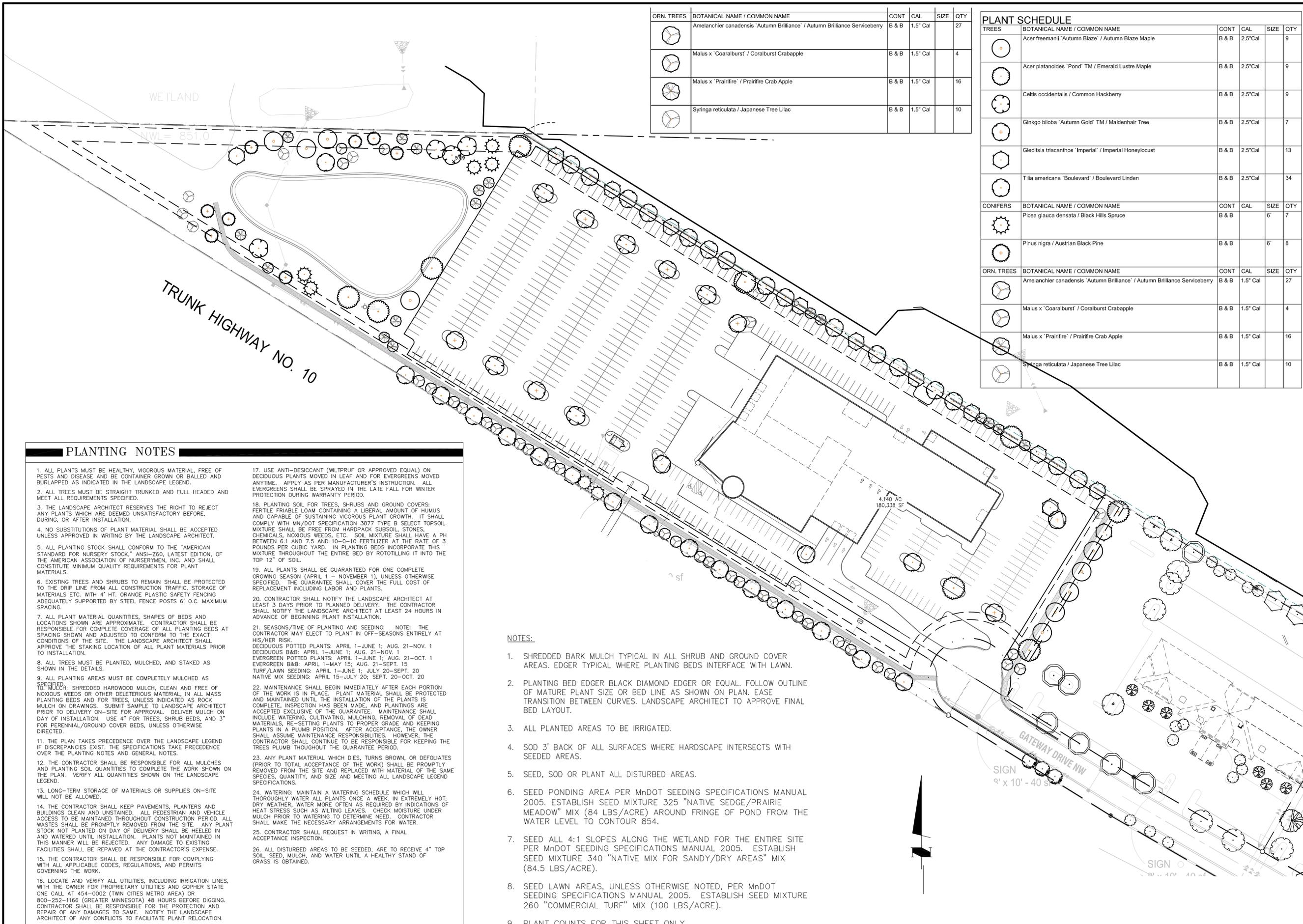
**Sheet No. Revision**  
**L1.02 I**

**Project No. H&W16148**

ORN. TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	SIZE	QTY
	Amelanchier canadensis 'Autumn Brilliance' / Autumn Brilliance Serviceberry	B & B	1.5" Cal		27
	Malus x 'Coaralburst' / Coralburst Crabapple	B & B	1.5" Cal		4
	Malus x 'Prairifire' / Prairifire Crab Apple	B & B	1.5" Cal		16
	Syringa reticulata / Japanese Tree Lilac	B & B	1.5" Cal		10

**PLANT SCHEDULE**

TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer freemanii 'Autumn Blaze' / Autumn Blaze Maple	B & B	2.5" Cal		9
	Acer platanoides 'Pond' TM / Emerald Lustre Maple	B & B	2.5" Cal		9
	Celtis occidentalis / Common Hackberry	B & B	2.5" Cal		9
	Ginkgo biloba 'Autumn Gold' TM / Maidenhair Tree	B & B	2.5" Cal		7
	Gleditsia triacanthos 'Imperial' / Imperial Honeylocust	B & B	2.5" Cal		13
	Tilia americana 'Boulevard' / Boulevard Linden	B & B	2.5" Cal		34
CONIFERS	BOTANICAL NAME / COMMON NAME	CONT	CAL	SIZE	QTY
	Picea glauca densata / Black Hills Spruce	B & B		6"	7
	Pinus nigra / Austrian Black Pine	B & B		6"	8
ORN. TREES	BOTANICAL NAME / COMMON NAME	CONT	CAL	SIZE	QTY
	Amelanchier canadensis 'Autumn Brilliance' / Autumn Brilliance Serviceberry	B & B	1.5" Cal		27
	Malus x 'Coaralburst' / Coralburst Crabapple	B & B	1.5" Cal		4
	Malus x 'Prairifire' / Prairifire Crab Apple	B & B	1.5" Cal		16
	Syringa reticulata / Japanese Tree Lilac	B & B	1.5" Cal		10



**PLANTING NOTES**

- ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
- ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
- THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
- NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
- ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
- EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4' HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6' O.C. MAXIMUM SPACING.
- ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
- ALL TREES MUST BE PLANTED, MULCHED, AND STAKED AS SHOWN IN THE DETAILS.
- ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
- MULCH: SHREDDED HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL, IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR TREES, SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
- THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN. VERIFY ALL QUANTITIES SHOWN ON THE LANDSCAPE LEGEND.
- LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
- THE CONTRACTOR SHALL KEEP PAVEMENTS, PLANTERS AND BUILDINGS CLEAN AND UNSTAINED. ALL PEDESTRIAN AND VEHICLE ACCESS TO BE MAINTAINED THROUGHOUT CONSTRUCTION PERIOD. ALL WASTES SHALL BE PROMPTLY REMOVED FROM THE SITE. ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAVED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND GOPHER STATE ONE CALL AT 454-0002 (TWIN CITIES METRO AREA) OR 800-252-1166 (GREATER MINNESOTA) 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
- USE ANTI-DESICCANT (WILTRUF OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WARRANTY PERIOD.
- PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MN/DOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDBACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5 AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED BY ROTOTILLING IT INTO THE TOP 12" OF SOIL.
- ALL PLANTS SHALL BE GUARANTEED FOR ONE COMPLETE GROWING SEASON (APRIL 1 - NOVEMBER 1), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
- CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
- SEASONS/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK.  
DECIDUOUS POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-NOV. 1  
DECIDUOUS B&B: APRIL 1-JUNE 1; AUG. 21-NOV. 1  
EVERGREEN POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-OCT. 1  
EVERGREEN B&B: APRIL 1-MAY 15; AUG. 21-SEPT. 15  
TURF/LAWN SEEDING: APRIL 1-JUNE 1; JULY 20-SEPT. 20  
NATIVE MIX SEEDING: APRIL 15-JULY 20; SEPT. 20-OCT. 20
- MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
- ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
- WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK. IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.
- CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.
- ALL DISTURBED AREAS TO BE SEEDDED, ARE TO RECEIVE 4" TOP SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.

**NOTES:**

- SHREDDED BARK MULCH TYPICAL IN ALL SHRUB AND GROUND COVER AREAS. EDGER TYPICAL WHERE PLANTING BEDS INTERFACE WITH LAWN.
- PLANTING BED EDGER BLACK DIAMOND EDGER OR EQUAL. FOLLOW OUTLINE OF MATURE PLANT SIZE OR BED LINE AS SHOWN ON PLAN. EASE TRANSITION BETWEEN CURVES. LANDSCAPE ARCHITECT TO APPROVE FINAL BED LAYOUT.
- ALL PLANTED AREAS TO BE IRRIGATED.
- SOD 3' BACK OF ALL SURFACES WHERE HARDSCAPE INTERSECTS WITH SEEDED AREAS.
- SEED, SOD OR PLANT ALL DISTURBED AREAS.
- SEED PONDING AREA PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 325 "NATIVE SEDGE/PRAIRIE MEADOW" MIX (84 LBS/ACRE) AROUND FRINGE OF POND FROM THE WATER LEVEL TO CONTOUR 85.4.
- SEED ALL 4:1 SLOPES ALONG THE WETLAND FOR THE ENTIRE SITE PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 340 "NATIVE MIX FOR SANDY/DRY AREAS" MIX (84.5 LBS/ACRE).
- SEED LAWN AREAS, UNLESS OTHERWISE NOTED, PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 260 "COMMERCIAL TURF" MIX (100 LBS/ACRE).
- PLANT COUNTS FOR THIS SHEET ONLY.



# Planned Unit Development (PUD) Design Guidelines

NW Corner Hanson Boulevard and Highway 10  
Coon Rapids, Minnesota

**Credits & Acknowledgements:**

We would like to thank the City of Coon Rapids planning staff, Planning Commission and City Council as well as the developer's engineers, architects and planners who attended the workshops and meetings and gave their input to shape the planning process. Their input has been invaluable in developing these design guidelines.

**City Council**

Tim Howe, Mayor  
Denise Klint, Ward 1  
Ron Manning, Ward 2  
Paul Johnson, Ward 3  
Joe Sidoti, Ward 4  
Bruce Sanders, Ward 5  
Scott Schulte, At Large

**Planning Commission**

Donna Naeve, Chair  
Tom Greenwood, Commissioner  
Denise Hosch, Commissioner  
Todd Mensink, Commissioner  
Margeret Murphy, Commissioner  
Joy Pirkel, Commissioner  
Jenny Geisler, Commissioner

**City of Coon Rapids**

Marc Nevinski, Community  
Development Director  
Scott Harlicker, Planner

**Developer & Consultants**

William Cooley, H & W Family LLLP  
Jonathan Adam, Windsong  
Properties, LLC.  
Mark Anderson, MFRA, Inc.  
Kathy Anderson, AIA,  
Architectural Consortium, LLC  
Brett Loftesnes, Architectural Consortium,  
LLC.

**Planning & Urban Design Consultant**

Carolyn Krall, AIA, Landform  
Ellen Stewart, ASLA, Landform  
Teresa Forsberg, Assoc. AIA, Landform

**Planned Unit Development (PUD) Site Development Agreement**

The Planned Unit Development (PUD) Design Guidelines are intended to be part of a PUD Site Development Agreement which, together with the approved Preliminary Plat, will define the project scope and act as the guiding plan for the development of this significant corner. The Guidelines are intended to work in conjunction with and in addition to the Coon Rapids City Code standards. All construction is subject to abide by all codes and ordinances as adopted by the State of Minnesota, Anoka County, the City of Coon Rapids and all other pertinent regulations. The design guidelines shall prevail in the event of a conflict.

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## **I. Introduction to PUD Design Guidelines**

- A. Purpose**
- B. Submissions, Review and Approvals Process**
- C. Introduction and Overview**

## **2. Commercial Design Guidelines**

### **A. General**

- 1) Land Use
- 2) Streets and Public Spaces
- 3) Open Space, Trails and Stormwater Management
- 4) Landscaping and Screening

### **B. Site Development**

- 1) Building Orientation
- 2) Site Coverage
- 3) Parking and Service Areas

### **C. Architecture**

- 1) Architectural Character
- 2) Building Massing and Form
- 3) Exterior Facades, Materials and Color Palette

### **D. Lighting and Signage**

- 1) Lighting
- 2) Signage

## **I. Introduction to P.U.D. Design Guidelines**

## A. Purpose

The Planned Unit Development (PUD) Design Guidelines are intended to be part of a PUD Site Development Agreement which, together with the approved Preliminary Plat, will define the project scope and act as the guiding plan for the development of this significant corner. The developer intends to develop the site in phases and may sell individual lots or parcels to developers, builders or owner-occupants who would complete the approvals and build-out of their individual lots. The guidelines ensure quality and consistency within the overall development through the process. Guidelines address site design and layout, harmonious style and materials, building massing, materials, architectural features, site landscaping, lighting and signage to establish an overall aesthetic for the Gateway Commerce Center.



The guidelines serve the purpose of identifying elements and design criteria that must be incorporated into the project and communicate the expectations of the City to individual site builders, owners and tenants for each phase within the overall development.

### Framework Principles for Coon Rapids

This Planned Unit Development should support the overall design principles for the City of Coon Rapids established in the Comprehensive Plan. These principles can

be summarized as follows:

- 1) Enhance Coon Rapids' image.
- 2) Improve the public realm.
- 3) Preserve and protect natural resources.
- 4) Encourage complementary, adjacent uses & assets.
- 5) Attract new, desirable uses.
- 6) Expand connectivity for both automobiles and pedestrians.
- 7) Reduce negative impacts of development (*i.e. traffic congestion, stormwater run-off, etc.*).

## B. Submission, Review and Approvals Process

These Design Guidelines will be finalized by City Staff and approved by the Planning Commission and City Council in conjunction with the Planned Unit Development (PUD) submission. This submission shall include all documentation itemized in the PUD Agreement for the entire development plan. The entire PUD shall be reviewed by the Planning Commission which will make recommendations to the City Council, who will consider approval of the entire PUD. The approved PUD submission documents, including the PUD Agreement and these Design Guidelines, will then form the basis for final site approvals for all or any part of the development. These guidelines are in effect and in conjunction upon approval of the final plat and PUD.

Final approvals for each individual lot within the PUD will go to the Planning Commission for review and final approvals, with appeal available to the City Council. However, proposed modifications which affect the entire PUD, such as road realignment or lot line adjustments, or any other substantial alterations to signage, lighting or landscape plans, will require City Council approval.

## C. Introduction and Overview

These Design Guidelines are intended to provide a framework for the future development of this significant site in Coon Rapids. The Design Guidelines should provide a baseline for quality

development that meets the community's goals.

**Goal 1:** Preserve and protect natural resources

**Goal 2:** Respect the existing context while encouraging new, contextual responses

**Goal 3:** Establish a baseline for design and quality

**Goal 4:** Provide a unified, overall plan and image to be consistent with phased development

## **Objectives**

The specific objectives for these Design Guidelines are:

- To establish a clear definition of urban design standards that informs property owners, developers, builders, tenants and current and future decision-makers
- To serve as a tool that planners and designers can use and incorporate into their plans to achieve and support the City's design objectives
- To act as a basis for a uniform and consistent review of development and redevelopment proposals by the City Council and Planning Commission

## **Format**

This document is written with Intent Statements, detailed Guidelines and Illustrations. The Intent Statements present the overall goal for development and describe a desired outcome within the site or building design category. The Guidelines provide specific directions on how the intent should be met. The Illustrations are either sketches that further illustrate the Guidelines or photos presenting examples of how the Design Intent has been achieved in other projects.

## **Definitions**

This document is written in common English with all terms as defined in the Merriam-Webster on-line dictionary ([www.m-w.com](http://www.m-w.com)). Land use terms shall be the same as defined in the City Code. The following terms are used frequently in the guidelines.

*context: the interrelated conditions in which something exists or occurs*

*discourage: to hinder by disfavoring*

*encourage: to attempt to persuade or to spur on*

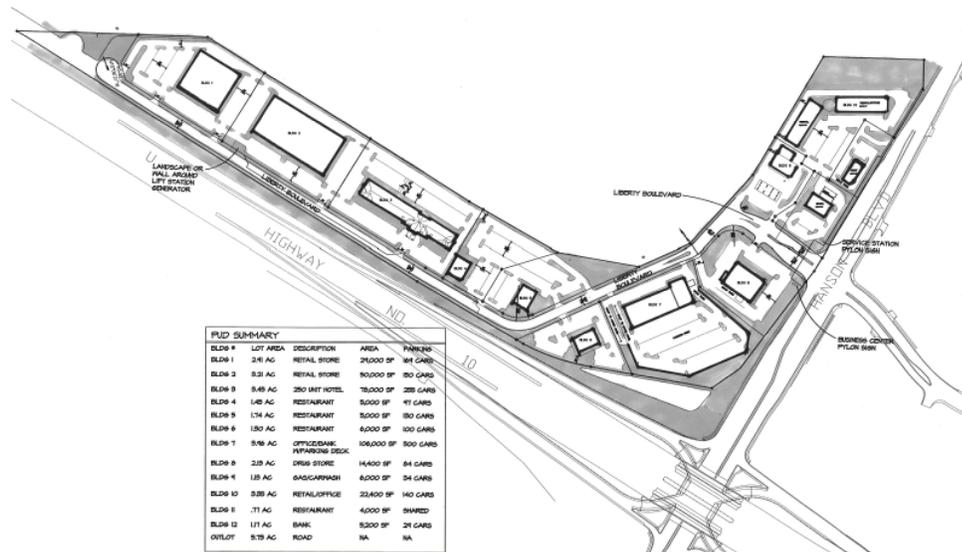
*shall: used in laws, regulations, or directives to express what is mandatory*

*should: used to express a request in a polite manner or to soften direct statement*

## **2. Commercial District**

### **A. General**

The Gateway Commerce Center is expected to include a mix of commercial uses – both destination uses and local, neighborhood-oriented retail and business uses. One side of the Center fronts on Highway 10 and has good visibility to support destination commercial uses. The other side fronts on Hanson Boulevard where more neighborhood-oriented commercial and smaller retail uses are appropriate. Hanson Boulevard offers the only vehicular access into the site. A full-access intersection is planned at Northdale Avenue and Hanson Boulevard which is relatively close to the Highway 10 ramp entrance. A right-in-only access, also on Hanson Boulevard, has been approved by Anoka County. The City hopes to be able to provide an additional road connection to the site from the northwest, enhancing the existing access significantly and allowing for a greater variety of commercial uses.



The Center is also adjacent to significant natural resources. North of the site there is an expansive wetland and in the future the City or County may undertake wetland restoration and improvements. Directly across the street from the Center is Lions Park – a local park with trails that are expected to be extended on the north side of the site to connect to the regional trail system. There are residential neighborhoods adjacent to the park and wetlands, so the Center should provide a pedestrian-friendly environment along this significant resource as well as pedestrian connections throughout the site.

1. New development should consider the available site access. Uses shall not exceed the assumptions included in the Traffic Study for the Business Center in Coon Rapids, December 2004, which formed the basis for the design of the Hanson Boulevard and Northdale intersection improvements. Uses which exceed those assumptions and would result in an overall intersection level of service (LOS) below D, measured at either peak AM or peak PM hour, at the Hanson Boulevard intersections are not acceptable.
2. All new development should be sensitive to existing residential in the design and location of new uses and buildings with respect to shading, views, light and noise pollution; where necessary, appropriate screening or buffering shall be provided.
3. New development shall provide for pedestrian as well as vehicular circulation and shall create linkages to existing and proposed sidewalks and trails.
4. The northwest corner of Hanson Boulevard and Highway 10 is a key point that has been identified as a primary entrance to Coon Rapids and should be designed as a “gateway” to the City. Gateway elements could include both City and development identity features, incorporating signage and unique landscaping, walls or gates, water features and open space which offer views to the adjacent wetlands and ponds.
5. New development should be designed to value, respect and preserve identified natural features and resources. Natural amenities shall be incorporated as features within public spaces through innovative planning and design approaches.



**1) Land Use**

Intent:

To welcome a mix of high quality office, hospitality, service and retail uses -- both destination uses, and local neighborhood retail and business uses. Land uses should be diverse yet complementary and provide places for residents to meet and socialize.



- a) Development should include a range of commercial uses appropriate to the Coon Rapids area including office, hospitality, retail, service and other commercial uses.
- b) Large scale commercial uses should be concentrated along the Highway 10 entrance ramp, rather than on Hanson Boulevard.
- c) A mix of diverse yet complementary uses appropriate to this area should be located closer to Hanson Boulevard; neighborhood-oriented retail and service commercial uses that support the community and nearby residential neighborhoods such as drug stores, banks, specialty markets and local retailers are encouraged.



- d) Retail or service uses whose primary use includes tattooing, branding, body piercing, or uses subject to licensing under Title 5 of the Coon Rapids City Code, except for the sale of alcoholic beverages or the operation of a therapeutic massage enterprise, shall not be allowed.



- e) Destination, hotel, restaurant, recreational and entertainment uses, particularly mixed with compatible, complementary or other synergistic uses, are encouraged. Automotive repair/service uses or uses with excessively high traffic generation are discouraged.
- f) Buildings 4, 5, and 6 will be “full-service, sit-down” restaurants, where food is both ordered and served by wait staff. Drive-through service will not be provided at these locations.
- g) Uses appropriate to this area that enhance their business by providing drive-through services (i.e. convenience food/gas stations, banks, coffee shops and pharmacies) are permitted provided they are designed to minimize impact on local traffic circulation and limit conflicts with pedestrian access.
- h) Parking lots shall be allowed only as an accessory use, except for shared ramp parking which shall be limited to serving on-site uses. Shared parking is encouraged (see section which follows on parking and services areas).



**2) Streets and Public Spaces**

Intent:

To provide a well designed public realm that combines access and safety for pedestrians and vehicles while enhancing connectivity between businesses, retail shops, restaurants, parking and public spaces through a logical, safe and well marked, vehicular and pedestrian circulation system.

- a) The central street through the development shall have a 60’ minimum right-of-way (ROW) which shall include the following:
  - (i) Drive lane width not more than 36 ft., west of building seven to reduce impervious surface, discourage larger trucks and encourage traffic to slow down, especially through the more active areas of the development;
  - (ii) Sidewalks not less than 5 ft. wide on both sides of the street at the Hanson Boulevard entrance. Sidewalks not less than 5 ft. wide on one side of the street in locations where there are uses only on one side of the street;
  - (iii) Planted boulevard buffer 6 ft. wide between the curb and sidewalk with street trees to provide shade and a sense of scale. This boulevard may be reduced or terminated approaching intersections to allow for pedestrian crossings;
  - (iv) Street lighting that meets the design standards and provides for both street lighting and pedestrian lighting on sidewalks to add character to the street corridor;
  - (v) Where appropriate, areas for seating, street furniture or other pedestrian amenities such as benches, planters, walls, fences, bollards, banner poles, bike racks and other elements that enhance the public realm. These elements shall be managed and maintained to reduce visual and physical clutter while elevating the quality of the pedestrian environment;



- (vi) Where necessary, such as where the street ROW does not allow for a wider sidewalk; these amenities may need to be accommodated within the parcel boundaries.
- b) All street intersections shall be safe for pedestrians and include enhancements to safety such as textured-colored paving or striping to mark crosswalks, narrowed or throated intersections, pedestrian-oriented street lighting, landscaped boulevards buffering the sidewalks from busy streets and pedestrian crossing warning signage at major crosswalks and all trail crossings.
- c) The development shall provide pedestrian connectivity between uses (businesses, parking, open space, etc.) with a designated and well marked pedestrian circulation system that encourages parking and walking rather than driving to each destination within the center, as designated in the pedestrian trail plan.

**3) Open Space, Trails and Stormwater Management**

Intent:

To enhance and protect adjacent, natural resources while offering the public enhanced access, connectivity and preserved views along the edge of the existing wetlands and creek.



- a) The development has received approvals for stormwater management and the required ponds, located outside the lot lines, are shown on the master plan. Pond edges and landscaping should appear as natural as possible, discourage geese, prohibit wading and reduce algae and other unattractive, surface accumulations.
- b) Site development should encourage the reduction of impervious cover and increase on-site infiltration to reduce potential non-point, source pollution and contaminants from stormwater runoff, where feasible.



- c) Maintain the minimum 10 ft. parking setback on the northern edge of the development along the wetlands as an open-space corridor connecting the site from north to south. The City intends to work with the County to seek approvals for a future pedestrian boardwalk and/or trail as a pedestrian amenity along the north edge of the property. To coordinate with this future amenity, the open space corridor should include:



- (i) At least two paved pedestrian connections between this corridor (future boardwalk) and the sidewalks along the central street, as appropriate;
- (ii) An eight foot wide paved bicycle trail connection extending from this corridor along the north side of the central street to the Hanson Boulevard intersection, where the City intends to connect the trail to the existing bicycle trail at Lions Park;



- (iii) Well defined pedestrian connections pedestrian-oriented destination uses such as retail, cafes or restaurants; these connections may be shared between lots or uses;

- (iv) Pedestrian amenities where the paved bicycle trail connects to this corridor that could include places to rest with benches, bike racks, pedestrian-oriented lights, banner poles, planters, shade trees and similar elements;
- (v) Landscaping that acts as a year-round buffer, and make a harmonious transition from the more groomed landscaped areas to the more natural landscape of the wetland edges.

**4) Landscaping and Screening**

Intent:

To provide landscaping that enhances the public realm, preserves natural areas including areas with wetlands and contiguous, significant native vegetation, maximizes wildlife habitat corridors and water quality and provides a focal point for development. Landscaping plans for the entire PUD and each individual site shall meet the requirements of the City’s landscaping standards for commercial developments to the extent possible and the goals of this section.



- (a) Where possible, utilize trees, shrubs and plant materials indigenous to this geographic location and soils, providing habitat value and promoting biodiversity through avoidance of monoculture plantings.



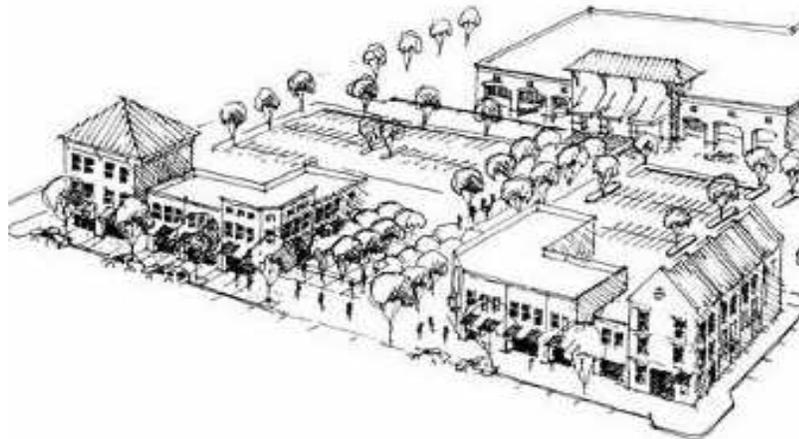
- (b) Maximize green space along the open-space corridor. Open space shall be planted with trees, shrubs, perennials and grasses to enhance the buffers for adjacent areas, reduce maintenance, reduce nitrogen usage and reduce water demands for irrigation while providing a focal point for development.

- (c) Provide street trees to frame the street and provide shade and scale with planting areas that are a minimum of 8 ft. long x 4 ft. wide to allow trees to grow to acceptable sizes. Structural soils (Cornell University Certified or equal) shall be the planting medium for all street trees.



- (d) Use a mixture of vegetation, berms, walls and fencing to screen areas such as parking lots, mechanical systems, transformers and loading docks. Screening materials shall be consistent with the building materials and other elements in the development. Fencing or a mechanical vent screen alone is not sufficient.

- (e) Off-street, surface parking areas shall include landscaped areas equivalent to 30 sq. ft. of landscaping for each parking space in the parking lot or not less than 10 percent of the area of the parking lot. Required landscaping shall take the form of planter strips, landscaped areas and perimeter landscaping. The landscaping shall be dispersed throughout the parking lot and must extend along the entire frontage of adjacent streets except for entry points. Planter strips shall be a minimum of five ft. in width.



- (f) Building facades and exposed foundations shall be articulated with landscaping to provide visual interest and soften appearances.
  - (i) Ornamental plant materials may be used such as flowering trees, shrubs and perennials.
  - (ii) Plantings shall be massed and scaled as appropriate for the building design.

## **B. Site Development**

The overall development plan should be well laid out to ensure that all uses have good access, well located parking, safe, attractive pedestrian connections and adequate visibility. The development should be pedestrian-friendly and walkable for everyone whether arriving by automobile, motorcycle, bicycle, bus or foot.

### **I) Building Orientation**

Intent:

To position buildings in a way that responds to climate and site conditions, provides good visibility to improve way-finding and enhances pedestrian accessibility.

- (a) All new development shall be sensitively designed and sited so as to preserve the key vistas from Hanson Boulevard through the site to the wetlands and creek.
- (b) Buildings shall be oriented and designed to consider sun and shade, wind exposure and snow drifting to avoid creating icy or dangerous conditions.
- (c) Building and paving setbacks shall be not less than 10 ft. on the wetland edge. Building setback shall not be less than 30 ft. from the Hanson Boulevard right of way and paving setbacks shall not be less than 10 ft. from the edge of the sidewalk easement.
- (d) Buildings along Hanson Boulevard shall be oriented to face the central parking area with well articulated facades. Service areas shall not face Hanson Boulevard.



Where commercial retail uses occupy the first floor, traditional storefronts (doors, display windows, or both) are encouraged to face towards the sidewalk.

- (e) Building entries shall be both visible and accessible by pedestrians from the main street (Hanson Boulevard or the new central street) and easily distinguished from the building facade; entrance areas shall be well lit, covered or protected from weather and include significant glazing and glass doors where the public will be entering the building.
- (f) Buildings may provide a second entrance from a side or rear parking lot, but the street-front entry must remain open during business hours.
- (g) Buildings shall be oriented to minimize views of service areas (service doors, equipment and loading docks) from public streets; access to these service areas needs to be provided at the alleys, sides or backs of the buildings.

## 2) Site Coverage

### Intent:

To minimize impervious surfaces and stormwater run-off by reducing building footprints, reducing paved parking areas and providing more permeable surfaces.



- (a) Building footprints and site coverage shall be minimized through the use of multiple stories and more efficient layouts where feasible.
- (b) Impervious surface or hardscape should be minimized by reducing the width of drive lanes and parking lots and encouraging central structured parking or shared parking where possible. The developer is responsible for demonstrating parking adequacy based on time of use and demands of individual and surrounding uses.
- (c) Alternative materials such as permeable pavers, porous asphalt, vegetated roof areas, especially on the parking deck, and other innovative techniques to reduce stormwater run-off shall be encouraged.

## 3) Parking and Service Areas

### Intent:

To encourage better site design including improved parking lot efficiency and ease of use while reducing the area of paved surface required and minimizing negative impacts of parking and service areas.

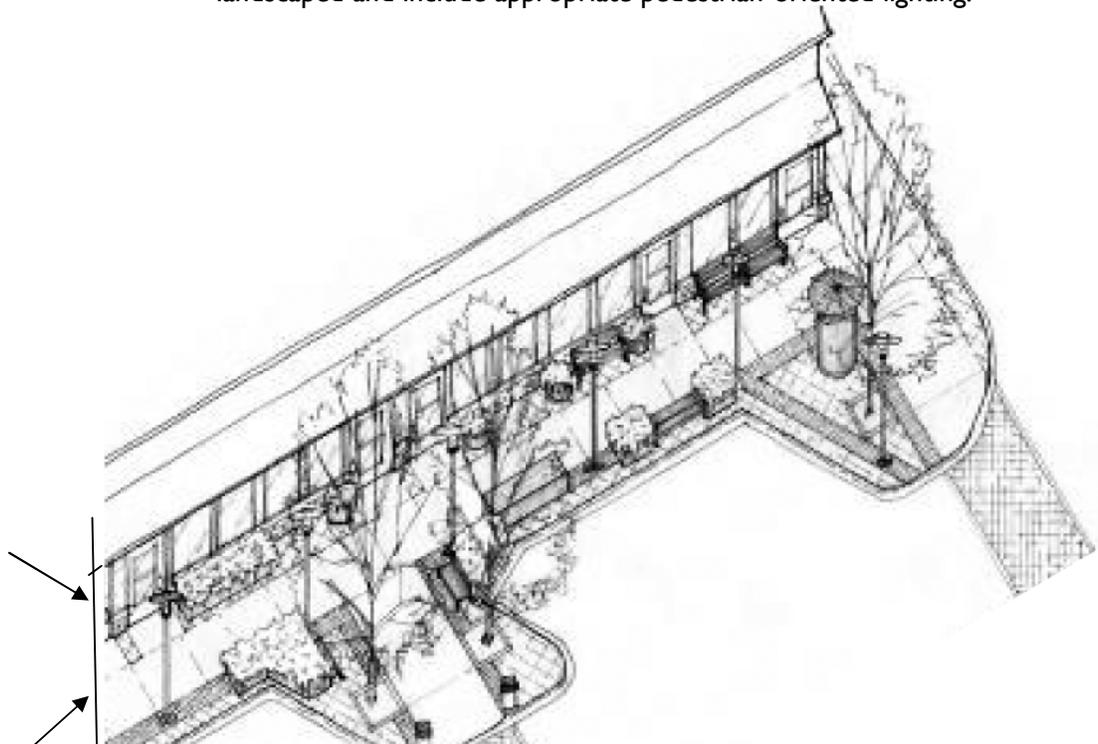


- (a) Parking lots shall be laid out for both efficiency and ease of use with entrances and exits clearly visible and marked.

- (b) Parking spaces should be a minimum of 9 ft. wide and 18 ft. long with a back-up area of not less than 24 ft. Up to 20% of the spaces provided in any parking lot may be designated for compact cars with parking spaces 16 ft. long.
- (c) Where appropriate and feasible, shared parking is encouraged to take advantage of varying parking demands between mixed uses and reduce the overall parking area on the site. The developer is responsible for demonstrating parking adequacy based on time of use and demands of individual and surrounding uses, using national standards for parking demand (such as ITE, APA, ULI, etc). If at any time, the planned uses change, the shared parking calculations would need to be reconsidered based on the proposed new uses.
- (d) Parking lots and driveways along Hanson Boulevard should be minimized. Where a driveway, parking lot or parking deck located along Hanson Boulevard should be well landscaped, screened and buffered with a combination of fencing, walls and plantings along the edge.
- (e) Where a parking lot fronts on the central street, Hanson Boulevard or the open space corridor, the lot shall be well landscaped and screened. Screening materials should include, singularly or in combination, ornamental fencing, walls, berms and plantings along the street edge.
- (f) Parking bays with an excess of 30 vehicle capacity shall be sub-divided with internal landscaping and pedestrian sidewalks or walkways to create the appearance of smaller parking areas. A bay consists of two rows of parking stalls nose to nose.
  - (i) Pedestrian walkways should be well defined (raised, paved and/or marked) and provide pedestrians a safe route to a sidewalk, trail, public space or the entrance of a commercial building.
  - (ii) Pedestrian walkways provided in larger parking lots should also be landscaped and include appropriate pedestrian-oriented lighting.

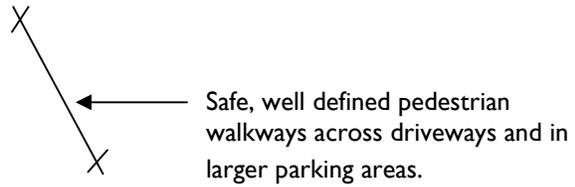


Shop-related furniture & amenities zone





Public Area:  
trash receptacles,  
newspaper stands,  
streetlights, signs  
& street trees  
planted where  
sidewalk widens



(g) Parking decks or structures shall not be located along or front on the open space corridor. Parking decks or structures fronting Hanson Boulevard shall be designed to minimize the apparent height of the ramp and the top parking deck shall not be more than 3 ft above the most adjacent Hanson Boulevard sidewalk elevation. Landscaped screening requirements are the same as for other parking lots (see above) along Hanson Boulevard.



(h) Buildings should be oriented to minimize views of service areas, service doors, meter banks and equipment and loading docks from the street. In addition, these areas should be screened with a combination of solid screening (walls or fences) and landscaping.



(i) Screening materials should be of the same style, materials and palette as the buildings; metal fencing or a mechanical vent screen alone is not sufficient.

(ii) Access to these service areas needs to be provided at the alleys, sides or backs of the buildings.



(i) All mechanical equipment and trash areas shall be fully screened and coordinated with the architecture of the buildings, using the same materials and construction type as the buildings. Free-standing enclosures are discouraged and interior service areas are encouraged.

**C. Architecture**

The architecture of the development and all new buildings should be respectful of and relate well to the existing context – natural and man-made – in massing, materials, style and color that expresses complementary design and an appropriate sense of scale.

**1) Architectural Character**

Intent:

To define the quality and character of the architecture for all new buildings and structures within the development.



- (a) Buildings should be designed to relate well to the existing natural and man-made environment in massing, materials, style and color. The design of the landscape and hardscape elements should be well coordinated and harmonious with the building design and materials.
- (b) Buildings should be designed in an appropriate, regional style using materials appropriate to the Coon Rapids area; the appearance of native stone is preferred (or a suitably durable material with an appearance of native stone) for landscape walls, monument signs, signage bases, architectural features, decorative hardscape elements and for a building base or foundation material.
- (c) Parking decks and their access ramps or structures shall be designed to harmonize with the buildings they serve in layout, massing, materials, color and overall architectural quality and character in order to minimize their visual impact.
- (d) Corporate chains shall customize their architecture as needed to express their design “brand” in a way that does not dominate the development and harmonizes with the overall development context, colors, materials and style.
- (e) Green building and site design and sustainable design, construction and maintenance practices are encouraged, where feasible.

**2) Building Massing and Form**

Intent:

To define the quality and character of the architecture for all new buildings and structures within the development.



- (a) The development shall allow for a variety of building heights and masses as appropriate and feasible for the proposed uses. Multi-story buildings are more appropriate along Highway 10 and close to the Highway 10 and Hanson Boulevard corner than directly on Hanson Boulevard. Building massing should encourage views out to, and over the wetlands amenity.
- (b) Building massing and design elements should include an expression of a building base (ideally with the appearance of stone), a middle and a top with an expressed cornice line at the top of the first floor and the top of the building.
- (c) Buildings should be designed using the same quality materials on sides that are visible from public streets. Rear or non-street facing facades shall include designs and materials that relate to the front or street-facing facades.
  - (i) All facades shall be articulated with a variety of materials, glazing, awnings or other details to add visual interest.



(ii) Roof lines and cornice details shall be completed in a three-dimensional manner so that the back of walls, roof features or any other unfinished areas are not visible.

(iii) Buildings may have expressed front façades with unique designs or more significant amounts of glazing, decorative elements or materials.



(d) Building design shall emphasize the pedestrian experience through the use of architectural features at the street level including awnings, canopies, stoops, porches, decorative light fixtures and material details to add scale and interest.

(e) Drive-through facilities shall have a roof or canopy cover which is integrated into the architectural design of the building or made of materials which complement the architectural design.

### 3) Exterior Facades, Materials and Color Palette

Intent:

To establish requirements for the exterior materials, finishes and overall palette which support the quality of architecture within the development.



(a) Buildings and site hardscape shall be constructed and maintained for permanence and longevity – durable, maintainable materials shall be used. Temporary buildings, metal or wood sheds, etc. shall not be permitted.

(b) Buildings shall be designed with quality materials on all sides and all facades shall be articulated with a variety of materials, glazing or details to add visual interest. Materials shall include a variety of colors and textures that complement each other.



(c) Buildings shall not have extensive areas of blank walls. Where windows or entrances are not possible, the architecture shall include varying walls, planes, materials, textures and colors to produce a better articulated and livelier building façade.

(d) Signage and lighting that are integrated into the design of the building are encouraged (*see next section*).

(e) Signage and lighting of corporate-branded buildings shall be designed to be integrated into the architecture of the building and the surrounding development, and not dominate the development.

**D. Lighting and Signage**

Lighting and signage for the Center should be appropriate for the uses planned and oriented as appropriate to the highway, the boulevard and pedestrian routes into and through the site. Lighting and signage shall be designed, sited and lit in ways that do not distract drivers, create safety concerns or cause undue impact on the adjacent park and residential neighborhoods.

**1) Lighting**

Intent:

There should be a coordinated approach to lighting throughout the site that is appropriate for the uses planned and designed to have minimal impact on adjacent uses. Pedestrian-oriented lighting will be required on all streets, trails and sidewalks and public gathering places within the Center.



(a) Lighting shall provide a well coordinated overall approach and incorporate both fixtures and lamps of similar types and colors throughout the development. A lighting plan and fixture specifications shall be included in the documents submitted to the City for approval.

(b) Architectural lighting, particularly decorative, accent lighting, is encouraged to highlight building character, massing and design features, particularly where the building meets the pedestrian realm.



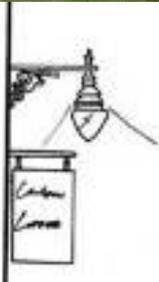
(c) Building entries with commercial or public uses shall be illuminated. Appropriate design approaches include washing the entry surfaces and doorways with light, allowing building interior light to glow through glazing, or using well designed and directed decorative lighting attached directly to the building.

(d) Buildings that front areas of high pedestrian activity, particularly along the wetland’s public edge, are encouraged to use identity lighting to enliven the space. Identity lighting includes accent lighting for the illumination of signs, awnings or graphics as well as interior lighting in storefront windows. Appropriate design approaches include decorative lighting fixtures attached directly to the building, focused spot lighting on signs, awnings, or window displays and interior decorative or identity lighted signs such as neon that are positioned to shine through glazing.



(e) Trails shall be provided with a minimum of lighting required for the safety of pedestrians, but it is generally assumed that park areas will not be used extensively after dark. Specific trail lighting is not required except where trails cross streets, access driveways, cross other trails or where pedestrian-oriented lighting should be provided for safe crossings.

(f) Street lights, standards and bases should be functional and, coordinated with the color palette, style and materials of the development, and be consistent throughout. Double-headed fixtures are recommended where both the street and pedestrian sidewalks are to be illuminated; otherwise, a single-headed fixture is acceptable. Fixtures and lamps shall be designed with a sharp cut-off angle and



shielded to direct light toward the ground to avoid shining unnecessary light upward to the sky.

- (g) Parking lots shall be lit with the same or similar style fixture as used for the street lights. Fixtures and lamps shall be designed for a wide-coverage area with a sharp cut-off to reduce both the number of fixtures required and glare of light spillage from parking areas.



- (h) All lighting fixtures, including spotlights, electrical reflectors and other means of illuminating signs, structures, landscaping, parking, loading and similar areas, shall be focused, directed and shielded to prevent horizontal glare, direct illumination of adjoining property or streets and upward light leakage.

- (i) Canopies shall not be lit, and lighting under covered drives or canopies shall not exceed the lighting levels appropriate for the functions and uses below; all fixtures shall be fully recessed, directed to reduce glare and screened to reduce visibility from the street. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center’s standard fixtures.

- (j) Lighting within parking decks shall not exceed the lighting levels appropriate for parking areas; all fixtures shall be located to reduce visibility from the street and screened to prevent glare. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center’s standard fixtures.

- (k) Security and service lighting shall be provided at all service areas and entries. Wherever possible, such lighting shall be controlled by a motion detector so that lighting is provided where and when it is needed for individual safety. Fixtures shall be building-mounted and designed to direct the light where needed and will not produce glare or light leakage. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center’s standard fixtures.

- (l) No mercury vapor utility lights or other light fixtures with high-intensity discharge lamps or bulbs that are not designed to limit, control light direction or shield the light source from view of neighboring residential properties shall be permitted.

**2) Signage**

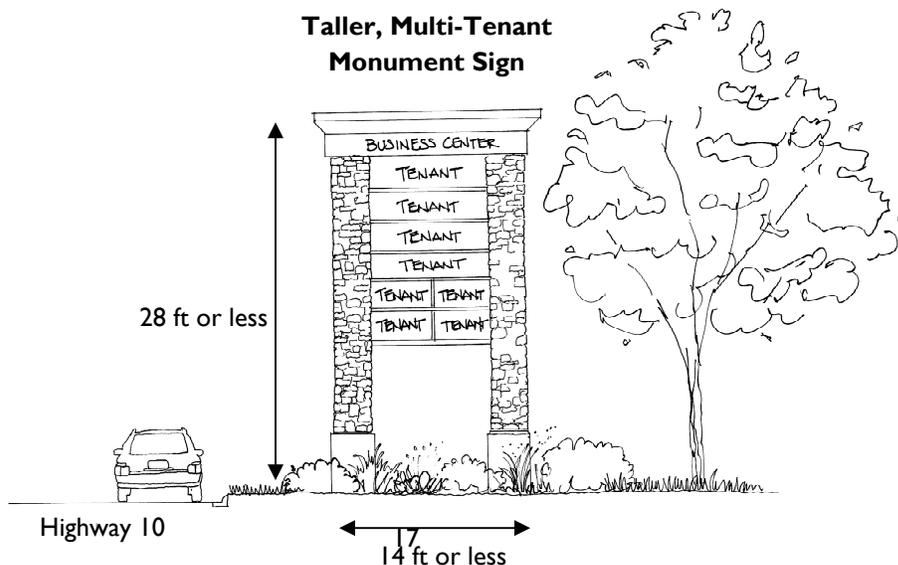
Intent:

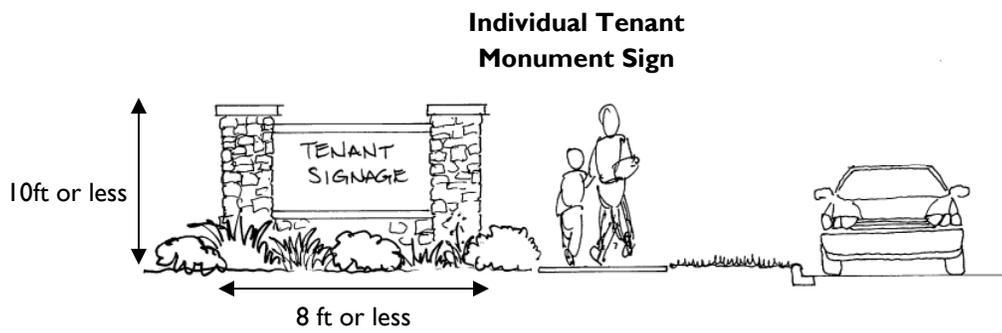
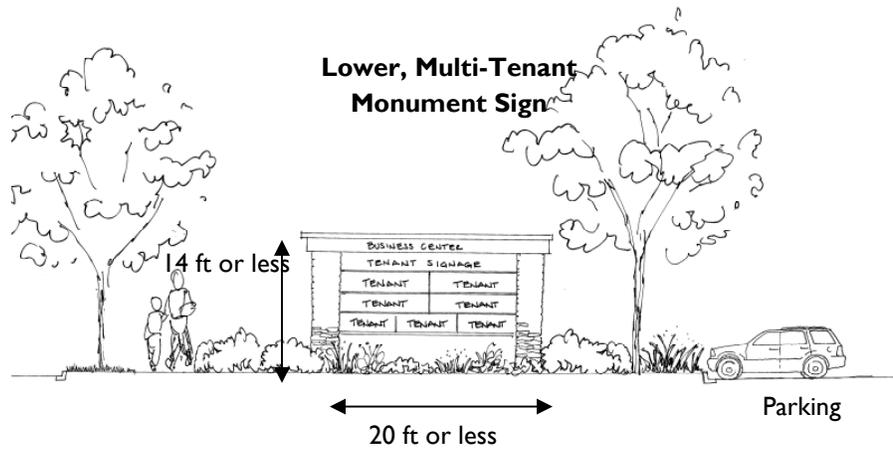
Signage is critical to both the character and the success of the Center, especially at night, so all signage (type, size, placement, materials, etc.) is subject to careful review by the City and must comply with the design guidelines as well as all zoning and signage ordinance requirements.

I.



- (a) All major site signage, as well as provisions for individual building signage and tenant signage, shall be included in the documents submitted to the City for overall site approval and in final approval submissions.
- (b) The Hanson Boulevard and Highway 10 corner is a prominent corner in the City and the Center shall locate an appropriate, welcoming identity element with signage, landscaping and lighting on the corner. This element may be integrated into the design of the corner building or parking ramp. Similar elements with signage, architectural elements, landscaping and lighting may be located at each street access or entrance to the site and shall be consistent with the primary corner feature.
- (c) The Center may locate appropriate monument identity signs on each street frontage; these monuments shall be of the same materials and color palette as the architecture of the Center, with the appearance of stacked native stone preferred. These signs may identify both the Center and individual tenants, as follows:
  - (i) Taller, multi-tenant monument sign located along the Highway 10 edge of the development (only one is allowed for the overall development, plus one such sign for a hotel); this sign shall be not more than 28 ft. in height and not more than 14 ft. in width and the actual back-lit sign area shall not exceed 180 sq. ft. per side in area. The sign shall be setback 50 ft. from the Highway 10 ROW;
  - (ii) Lower multi-tenant monument sign located on Hanson Boulevard (not more than two are allowed); this sign shall be less than 14 ft. in height and less than 20 ft. in width, and the actual back-lit sign area shall not exceed 140 sq. ft. in area per side;
  - (iii) Individual tenant signs (not more than one per lot is allowed); this sign shall be less than 10 ft. in height and less than 8 ft. in width, and the actual back-lit sign area shall not exceed 50 sq. ft. in area per side. These signs shall be located on the same lot as the tenant. However, the convenience store is allowed to locate its tenant sign along Hanson Boulevard.







- (d) Monument signs shall be spaced not less than 100 ft. apart; and shall be placed so that they do not impact safety by blocking motorist views of pedestrian crossings or oncoming traffic at intersections and driveways.
- (e) All monument signs shall be set back 10 ft. from any street ROW.
- (f) Exterior signage shall be fixed and stationary with no electronic, flashing, changing or moving elements. *(coordinate with City policy re: moving signs)*
- (g) Building signage and graphics shall be integral to the building architecture or used as a decorative, design element. More traditional awnings, hanging signs or architecturally integrated signs that are lit by traditional lighting fixtures (spots, sconces, etc.) are encouraged. Large areas of back-lit signage are discouraged and back-lit awning or canopy signs are not acceptable.
- (h) Interior signage, such as decorative neon, moving or other decorative sign types are desirable in limited size when displayed within storefront windows in the interiors of buildings.
- (i) Corporate chains are encouraged to customize their architecture to express their design “brand” within a design, context and material appropriate to the character of the development and should not rely solely on corporate signage. Corporate signage shall be high quality and well integrated into the design of the buildings in a way that does not detract from the overall design of the development.
- (j) The development shall provide both auto and pedestrian way-finding signs and other environmental graphics to supply directions to the pedestrian. This shall be done in a comprehensive and unified manner for the entire development. The proposed graphics should be included in the submission documents for final approvals.

[End of Design Guidelines]



## City Council Work Session

2.

**Meeting Date:** 05/24/2016

**Subject:** Consider Phase One Boulevard Park Layout

**Submitted For:** Tim Himmer, Public Works Director

**From:** Sarah Greene, Administrative Assistant II

---

### **INTRODUCTION**

The City Council is asked to review and comment on the attached park layout concepts for Boulevard Park.

### **DISCUSSION**

At the 2015 City Council/Staff retreat, Boulevard Park was prioritized as the next park improvement to plan and construct (2017) under the approved park bond program. Boulevard Park is the area in front of the Coon Rapids Ice Center (CRIC), adjacent to Coon Rapids Blvd. in the approximate location of the former Firestone building.

Since this area is also part of the PORT Campus Square redevelopment area and master plan, staff presented several spatial concepts for City Council consideration in work session on March 8, 2016. In general terms, options 4 and 5 of the attached concepts were selected as "preferred." There was discussion, however, that the final spatial configuration would be a hybrid, and any mixed use/retail component on the south side of Coon Rapids Blvd should be eliminated.

The proposed 2017 park development project would be contained within the orange "play" area on the spatial concepts provided. Council direction at the March meeting was to provide flexible open space that could be identified and used as the community gathering space, and accommodate all city events in one location (4th of July, movies/concerts in the park, farmers market, etc.). The yellow "plaza" space was also identified as the location for a bandshell or elevated gazebo structure of some sort, which would come at a later date and be integrated into the "play" area to allow for the planning of desired community events.

When this park was initially included in the Park Bond Referendum it included a splash pad, picnic space/seating area, and a play area at an estimated cost of \$600,000. A rough concept plan was provided at that time, which included a flexible phase 1 project that allowed for future expansion as the PORT was redeveloped. Based upon Council input on March 8th, that concept has since been refined and is attached as Layout 1. Staff also contacted a playground company and asked if they could provide a layout that included all the desired amenities in a fully accessible "destination" type park, and this is attached as Layout 2.

## **RECOMMENDATION**

Staff is seeking Council feedback on the layouts provided to assist in defining the ultimate scope of improvements. If desired, this project could advance to the design phase and be ready for construction following the 4th of July event in 2017.

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## **Attachments**

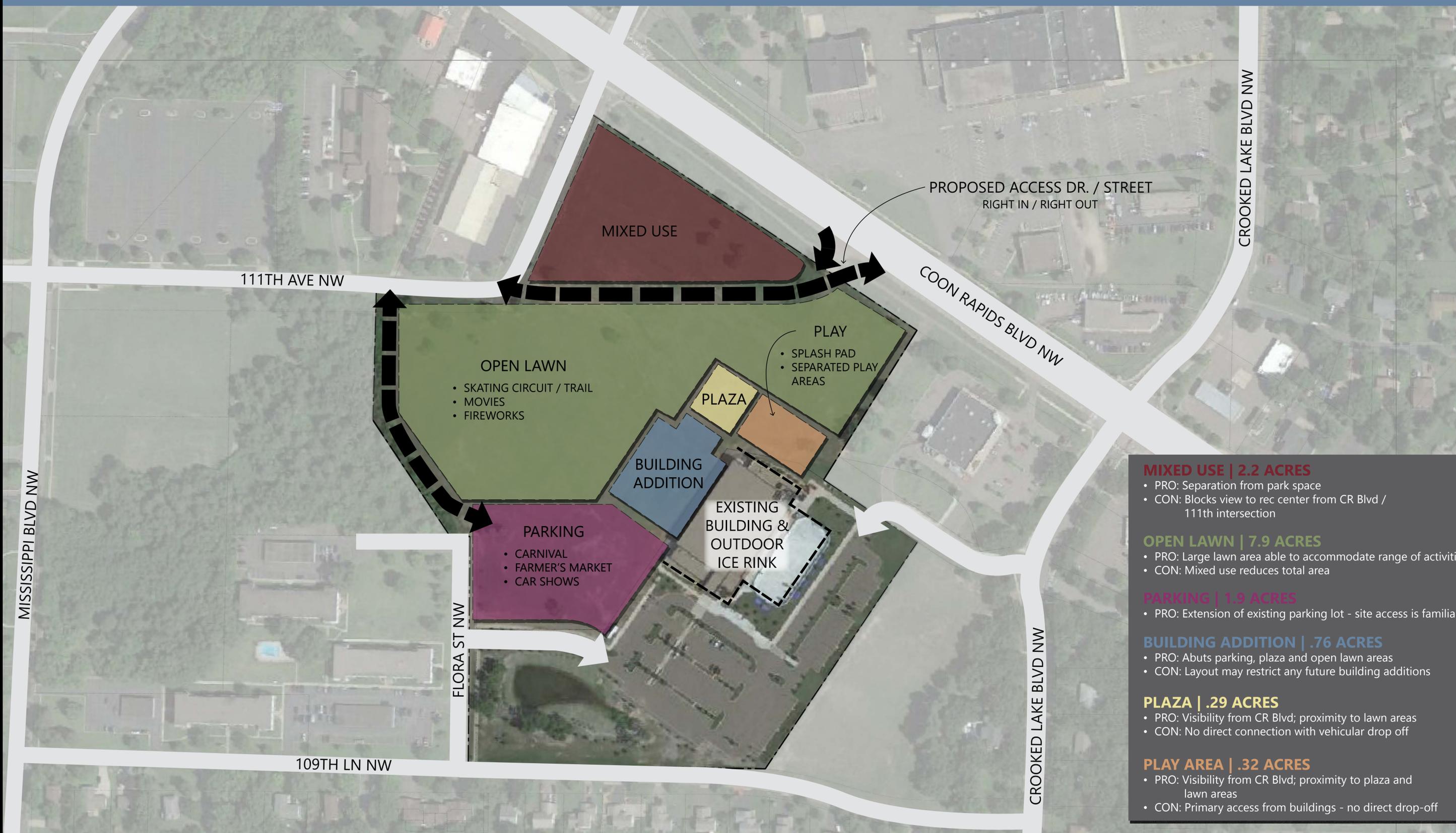
Spatial Concepts

Layout 1

Layout 2

Layout 2 Rendering

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- MIXED USE | 2.2 ACRES**
- PRO: Separation from park space
  - CON: Blocks view to rec center from CR Blvd / 111th intersection
- OPEN LAWN | 7.9 ACRES**
- PRO: Large lawn area able to accommodate range of activities
  - CON: Mixed use reduces total area
- PARKING | 1.9 ACRES**
- PRO: Extension of existing parking lot - site access is familiar
- BUILDING ADDITION | .76 ACRES**
- PRO: Abuts parking, plaza and open lawn areas
  - CON: Layout may restrict any future building additions
- PLAZA | .29 ACRES**
- PRO: Visibility from CR Blvd; proximity to lawn areas
  - CON: No direct connection with vehicular drop off
- PLAY AREA | .32 ACRES**
- PRO: Visibility from CR Blvd; proximity to plaza and lawn areas
  - CON: Primary access from buildings - no direct drop-off



# BOULEVARD PARK

concept one





- MIXED USE | 1.4 ACRES**
  - PRO: Proximity to intersection - good access and visibility
  - CON: Blocks view to center from CR Blvd / 111th intersection
- OPEN LAWN | 9.7 ACRES**
  - PRO: Large area to accommodate a wide range of activities
  - CON: Impacted by shared parking near retail
- PARKING | 1.9 ACRES**
  - PRO: Extension of existing parking lot - site access is familiar
  - CON: No additional entrance from north side of site
- SHARED PARKING | .42 ACRES**
  - PRO: Lets visitors access site from north
  - CON: Separated parking areas; Impacts greenspace
- BUILDING ADDITION | .76 ACRES**
  - PRO: Abuts parking, plaza and open lawn areas
  - CON: Layout may restrict any future building additions
- PLAZA | .29 ACRES**
  - PRO: Visibility from CR Blvd; proximity to lawn and play areas
  - CON: No direct connection with vehicular drop off
- PLAY AREA | .32 ACRES**
  - PRO: Visibility from CR Blvd; proximity to plaza and lawn areas
  - CON: Primary access from buildings - no direct drop off



# BOULEVARD PARK

concept two





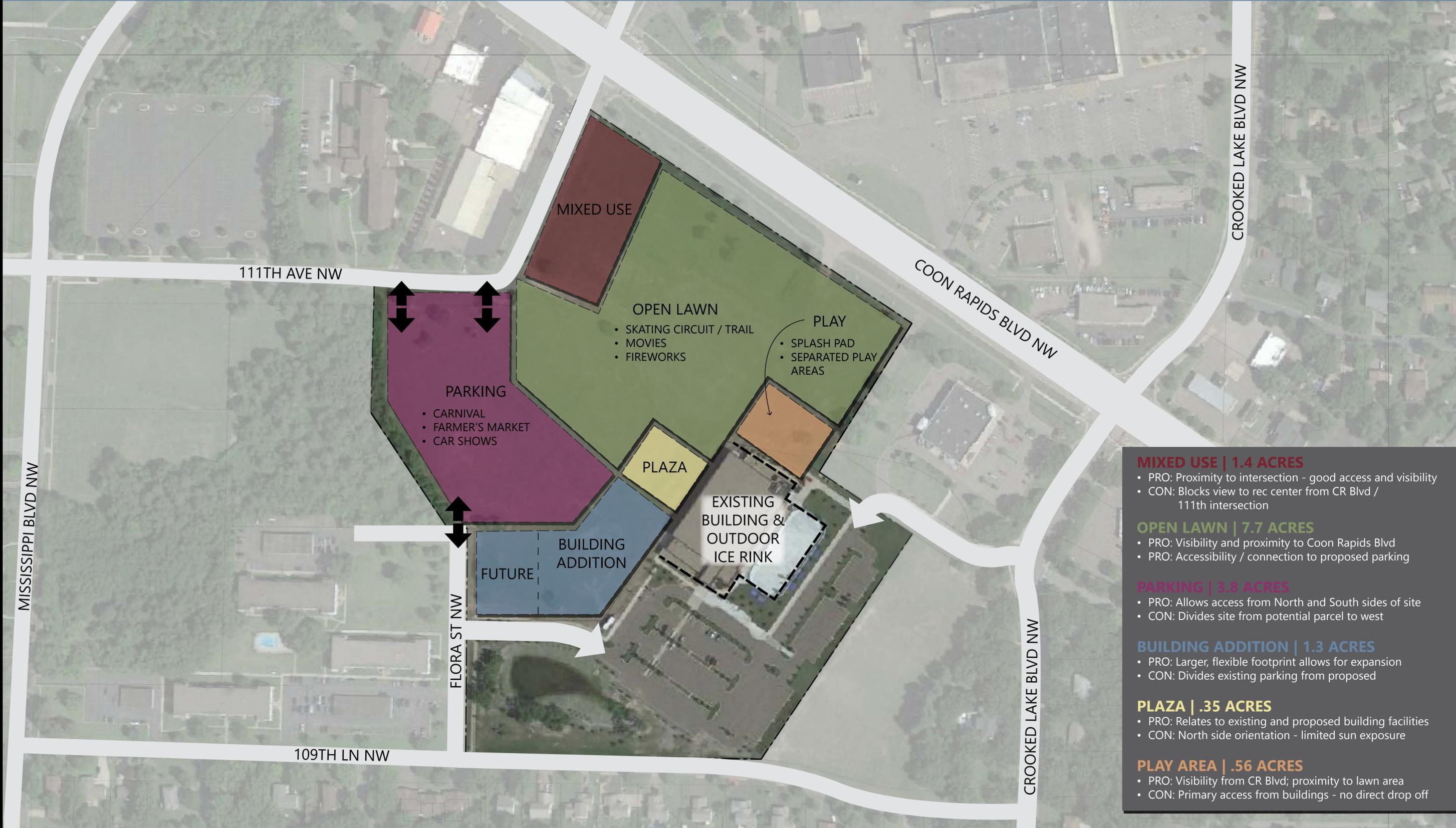
- MIXED USE | 1.7 ACRES**
  - PRO: Relationship with existing retail use
  - CON: Introduces more traffic through park site
- OPEN LAWN | 12.9 ACRES**
  - PRO: Visibility and proximity to CR Blvd /111th intersection
  - CON: Lawn area divided by proposed access road
- PARKING | 1.6 ACRES**
  - PRO: Extension of existing parking lot - site access is familiar
  - CON: No access from north side of site to main parking area
- SHARED PARKING | .83 ACRES**
  - PRO: Lets visitors access from north side of site
  - CON: Facility loses the "front yard" feel along CR Blvd
- BUILDING ADDITION | .76 ACRES**
  - PRO: Abuts parking, plaza and open lawn areas
  - CON: Layout may restrict any future building additions
- PLAZA | .27 ACRES**
  - PRO: South Sun exposure; direct connection to parking
  - CON: No visibility from CR Blvd
- PLAY AREA | .28 ACRES**
  - PRO: South sun exposure; direct access from parking/plaza
  - CON: Less visibility from CR Blvd.



# BOULEVARD PARK

concept three





- MIXED USE | 1.4 ACRES**
  - PRO: Proximity to intersection - good access and visibility
  - CON: Blocks view to rec center from CR Blvd / 111th intersection
- OPEN LAWN | 7.7 ACRES**
  - PRO: Visibility and proximity to Coon Rapids Blvd
  - PRO: Accessibility / connection to proposed parking
- PARKING | 3.8 ACRES**
  - PRO: Allows access from North and South sides of site
  - CON: Divides site from potential parcel to west
- BUILDING ADDITION | 1.3 ACRES**
  - PRO: Larger, flexible footprint allows for expansion
  - CON: Divides existing parking from proposed
- PLAZA | .35 ACRES**
  - PRO: Relates to existing and proposed building facilities
  - CON: North side orientation - limited sun exposure
- PLAY AREA | .56 ACRES**
  - PRO: Visibility from CR Blvd; proximity to lawn area
  - CON: Primary access from buildings - no direct drop off



# BOULEVARD PARK

concept four





**OPEN LAWN**

- SKATING CIRCUIT / TRAIL
- MOVIES
- CARNIVAL
- CAR SHOWS
- FIREWORKS

**PLAY**

- SPLASH PAD
- SEPARATED PLAY AREAS

**PLAZA**

**BUILDING ADDITION**

**PARKING**

- CARNIVAL
- FARMER'S MARKET
- CAR SHOWS

**EXISTING BUILDING & OUTDOOR ICE RINK**

**OPEN LAWN | 9.4 ACRES**

- PRO: Large area to accommodate a wide range of activities
- CON: Lawn area somewhat divided by plaza and play area

**PARKING | 2.7 ACRES**

- PRO: Extension of existing parking lot - access is familiar
- PRO: Additional access from north side of site

**BUILDING ADDITION | .76 ACRES**

- PRO: Abuts parking, plaza and play areas

**PLAZA | .34 ACRES**

- PRO: Relates to parking, play area and proposed building
- CON: Does not abut existing building

**PLAY AREA | .17 ACRES**

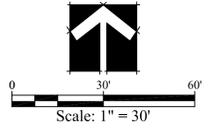
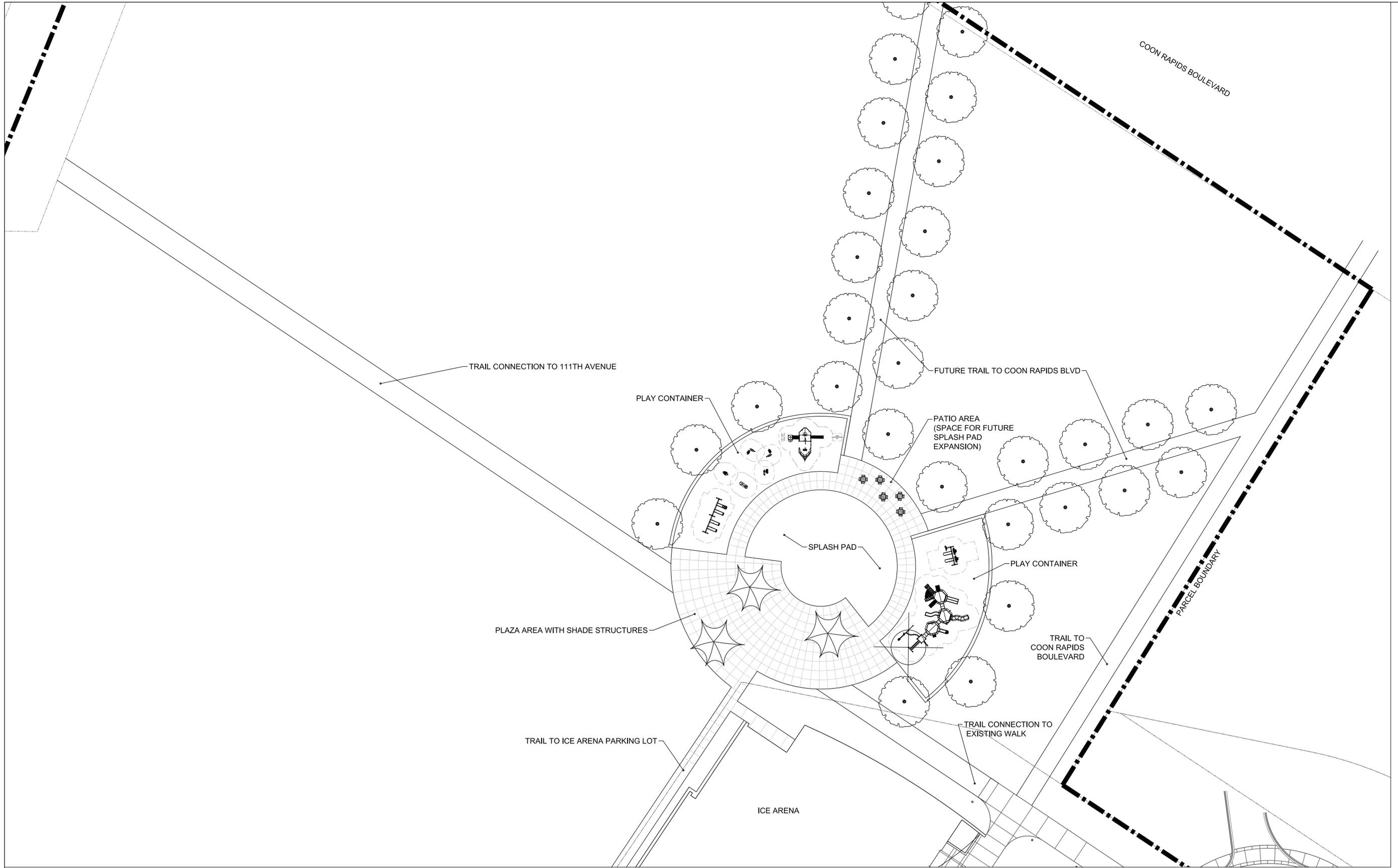
- PRO: Visibility from CR Blvd; proximity to plaza and lawn areas
- CON: Primary access from buildings - no direct drop off



# BOULEVARD PARK

concept five





I HEREBY CERTIFY THAT THE PLAN, SPECIFICATION, AND REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED REGISTERED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA

REVISION NO.	AS NOTED	EXPLANATION
DATE	DESIGN BY:	
	PROJECTING:	
	CHECKED BY:	
	RECORD COPY BY:	
	DATE	

DATE: Apr. 8, 2016 printed name: \_\_\_\_\_ LIC. NO.: \_\_\_\_\_

**City of Coon Rapids  
Boulevard Park  
Play Area Concept Plan**

701 Xenia Avenue South, Suite 300  
 Minneapolis, MN 55416  
 Tel: (763) 541-4800 • Fax: (763) 541-1700  
 wsbeng.com

**WSB**  
 & ASSOCIATES, INC.





**Boulevard Park**  
Coon Rapids, Minnesota



## City Council Work Session

3.

**Meeting Date:** 05/24/2016

**Subject:** 2018 Comprehensive Plan Update

**From:** Grant Fernelius, Community Development Director

---

### INTRODUCTION

Every 10 years cities within the metro area are required to update their Comprehensive Plans. The purpose of this exercise is to ensure that future growth and development is not only orderly, but consistent with the region-wide system/policy plans of the Met Council for such things as transportation, waste water, water supply and parks/trails. On Tuesday evening, staff will provide an overview of the process for updating the City's Comprehensive Plan, which will begin this summer and must be completed by December 31, 2018. In advance of the discussion, staff thought it would be helpful to provide some background information on the topic. We are also seeking feedback from Council on a range of issues, including the scope of the update, role for advisory commissions, public input and the overall process. Additional details are discussed in this memorandum.

### DISCUSSION

The City's current Comprehensive Plan was adopted in 2008 and is focused on long-range planning through 2030. A link to the current Comp Plan can be found here: <http://mn-coonrapids.civicplus.com/436/Comprehensive-Plan>. The document incorporates all of the elements required under state law, including basic demographic information, land use, transportation, water supply, sanitary and storm water systems, parks and open space, solar access protection, housing and wetlands protections.

Among the key assumptions at the time, was that the City would experience a mixed amount of growth between 2010 and 2030. For example, the population was expected to remain flat, while the number of households and employment would grow slightly. Other trends highlighted an aging population, fewer families with children, an aging housing stock and the growth of "upstream" communities. Many of these trends have become reality in the last few years. The Met Council has prepared new forecasts which project additional growth both regionally and in Coon Rapids. Thus, there is a real need to update the current Comp Plan.

### Thrive MSP 2040 and System Statements

The foundation for the next round of updates starts with regional forecasts and development planning. In May 2014, the Met Council adopted a new regional development guide known as *Thrive MSP 2040*, which identified a series of desired outcomes for the region over the next

several decades. Among the key predictions is that the region will add 824,000 residents by 2040 – a 29% increase from 2010. In addition, the seven-county region is expected to add more than 550,000 jobs (+36%) during this same time period. To accommodate all of this growth, the Met Council updated its system/policy plans and issued new system statements for all communities in the region. The statements were distributed to cities in September 2015. A copy of the City's system statement is attached. As an aside, last October staff provided a copy of this document to Council in response to some questions about the housing policy component.

In the 2015 System Statement, Coon Rapids is designated as a Suburban community, which recognizes that much of our growth and development occurred during the 1980's and 1990's and has been more automobile-oriented with lower densities than older parts of the region. The regional plan assumes that communities like Coon Rapids will develop at densities of 5 units per acre and target growth opportunities near regional transit investments. For example, the recent discussions about development at the Riverdale TOD site and PORT Riverwalk align very well with these goals.

For Coon Rapids, the Met Council is forecasting that population, households and employment will all increase between 2020 and 2040 as shown in the table below.

	2010		2014		2020		2030		2040		Total
Population	61,476	62,435	1.6%	64,800	3.8%	68,400	5.6%	72,100	5.4%	15.5%	
Households	23,532	23,905	1.6%	25,500	6.7%	27,500	7.8%	29,300	6.5%	22.6%	
Employment	23,260	25,483	9.6%	27,100	6.3%	28,900	6.6%	30,900	6.9%	21.3%	
Note: % change reflects increase over previous period.											

The 2015 System Statement describes how these future projections will impact housing, transportation, water resources and regional park systems. We expect to have more discussion in the coming months about incorporating these issues into the 2018 Comp Plan.

### 2018 Comp Plan

The 2018 Comp Plan will include many of the same elements as the current plan. State law requires cities to address land use, transportation, water resources, parks and trails, housing and plan implementation in their comp plans. In addition, the Met Council is encouraging cities to consider two new elements: *resilience* (i.e. environment, energy, sustainability, healthy communities, etc.) and *economic competitiveness* (redevelopment, education, workforce, business development). Staff believes there is merit to adding these elements to the 2018 Comp Plan, given the recent discussions on economic development and environmental sustainability. We have also attached a time line with key dates for your review and comment.

### Resources

We are anticipating that City staff will lead the update effort (i.e. reviewing the document, collecting basic data, revising/adding text, updating maps and exhibits, etc.). However, we may need outside assistance for portions of the work, particularly for any technical or specialized needs (i.e. market analysis, transportation modeling, etc.). In addition, if we want to include broader public participation, design charrettes or other formal events it would be helpful to hire a consultant to assist with those tasks. Coincidentally, the Met Council recently authorized grant assistance to 43 communities in the metro area. Preliminary information indicates that Coon Rapids is eligible to receive a grant of \$32,000 to help update our Comp Plan. More details will be available in the next few weeks. We are also planning to assemble an interdepartmental team to assist with various components of the Comp Plan update. Community Development staff will take the lead in managing the process.

## Feedback

A key outcome of the work session on Tuesday night is to get Council feedback on several issues, which are highlighted below:

**Visioning vs. Technical Update:** The Comp Plan should function as more than a document to meet statutory requirements. Ideally, it reflects the community's vision for the future and serves as a road map for how to achieve desired outcomes. There has been a great deal of planning and community participation over the years, especially along Coon Rapids Boulevard and to some extent the City is still in the process of working on the goals identified in the 2008 Comp Plan. Assuming these goals are still relevant, one option is to focus on gathering new data and modifying the plan to show what has been accomplished to date and what needs to be done in the years ahead. Another option would be to take a step back and re-assess the goals or set new ones for the future. This approach would take more and likely involve a number of different stakeholders, but is possible to do within the proposed time frame.

**Role of Council and Advisory Commissions:** There will be several opportunities to include the City's advisory commissions in the Comp Plan process. At a minimum, the Planning Commission would be involved, although other commissions could provide value to the process. It will also be important to understand the role the Council wants to play in the process too. For example, the Council could choose to wait for the public to weigh in and review the finished product or be involved in the process along the way. The proposed time line has identified two key points for a formal check-in with the Council. However, that can be adjusted based on the Council's preferences.

**Opportunity for Public Input:** Citizen input will be important to making the Comp Plan a legitimate document. At a minimum, we would want to hold an Open House or use the Summer in the City meetings (2017) as opportunities for feedback. However, we may also want to include other civic groups too. Any suggestions would be greatly appreciated. Lastly, we will need to do a significant amount of communication, education and outreach to the public. This would take place through multiple formats (i.e. website, social media, newsletter, press releases, cable TV, etc.). One of the challenges may be reaching out those parts of our community that have not traditionally participated in these kinds of efforts. We'll need to be open to new ideas for connecting with the public.

**RECOMMENDATION**

No formal action is requested at this time.

---

**Attachments**

2015 System Statement

2018 Comp Plan Update Timeline

---

# 2015 SYSTEM STATEMENT

System Statement Issue Date:

# 2015 SYSTEM STATEMENT FOR CITY OF COON RAPIDS

September 17, 2015

## Regional Development Plan Adoption

In May 2014, the Metropolitan Council adopted *Thrive MSP 2040*. Following adoption of *Thrive*, the Council adopted the *2040 Transportation Policy Plan*, the *2040 Regional Parks Policy Plan*, the *2040 Water Resources Policy Plan*, and the *2040 Housing Policy Plan*. The Metropolitan Council is now issuing system statements pursuant to [State statute](#).

Receipt of this system statement and the metropolitan system plans triggers a community's obligation to review and, as necessary, amend its comprehensive plan within the next three years, by the end of 2018. The complete text of *Thrive MSP 2040* as well as complete copies of the recently adopted metropolitan system and policy plans are available for viewing and downloading at <http://www.metrocouncil.org/Communities/Planning.aspx>. Paper copies are available by calling the Council's Data Center at 651-602-1140.

## System Statement Definition

Metropolitan system plans are long-range comprehensive plans for the regional systems – transit, highways, and airports; wastewater services; and parks and open space – along with the capital budgets for metropolitan wastewater services, transportation, and regional recreation open space. System statements explain the implications of metropolitan system plans for each individual community in the metropolitan area. They are intended to help communities prepare or update their comprehensive plan, as required by the Metropolitan Land Planning Act:

*Within nine months after receiving a system statement for an amendment to a metropolitan system plan, and within three years after receiving a system statement issued in conjunction with the decennial review required under section [473.864, subdivision 2](#), each affected local governmental unit shall review its comprehensive plan to determine if an amendment is necessary to ensure continued conformity with metropolitan system plans. If an amendment is necessary, the governmental unit shall prepare the amendment and submit it to the council for review.*

Local comprehensive plans, and amendments thereto, will be reviewed by the Council for conformance to metropolitan system plans, consistency with Council policies, and compatibility with adjacent and affected governmental units. Updated local comprehensive plans are due to the Council for review by December 31, 2018.

## What is in this System Statement

The system statement includes information specific to your community, including:

- your community designation or designation(s);
- forecasted population, households, and employment through the year 2040;
- guidance on appropriate densities to ensure that regional services and costly regional infrastructure can be provided as efficiently as possible.
- affordable housing need allocation;

In the following sections, this system statement contains an overview of each of the system plan updates and specific system changes that affect your community. The sections are:

- Transportation, including metropolitan highways, aviation, and transit
- Water Resources, including wastewater, surface water, and water supply planning
- Regional parks and trails

## **Dispute Process**

If your community disagrees with elements of this system statement, or has any questions about this system statement, please contact your Sector Representative, Eric Wojchik, at 651-602-1330, to review and discuss potential issues or concerns.

The Council and local government units and districts have usually resolved issues relating to the system statement through discussion.

## ***Request for Hearing***

If a local governmental unit and the Council are unable to resolve disagreements over the content of a system statement, the unit or district may, by resolution, request that a hearing be conducted by the Council's Land Use Advisory Committee or by the State Office of Administrative Hearings for the purpose of considering amendments to the system statement. According to Minnesota Statutes section 473.857, the request shall be made by the local governmental unit or school district within 60 days after receipt of the system statement. If no request for a hearing is received by the Council within 60 days, the statement becomes final.

## Regional Development Guide

The Council adopted [Thrive MSP 2040](#) as the new regional development guide on May 28, 2014. *Thrive* identifies five outcomes that set the policy direction for the region's system and policy plans. Building on our region's history of effective **stewardship** of our resources, *Thrive* envisions a **prosperous, equitable, and livable** region that is **sustainable** for today and generations to come. The Council is directing its operations, plans, policies, programs, and resources toward achieving this shared long-term vision.



Three principles define the Council's approach to implementing regional policy: **integration, collaboration, and accountability**. These principles reflect the Council's roles in integrating policy areas, supporting local governments and regional partners, and promoting and implementing the regional vision. The principles define the Council's approach to policy implementation and set expectations for how the Council interacts with local governments.

*Thrive* also outlines seven land use policies and community designations important for local comprehensive planning updates. The land use policies establish a series of commitments from the Council for local governments and uses community designations to shape development policies for communities. Community designations group jurisdictions with similar characteristics based on Urban or Rural character for the application of regional policies. Together, the land use policies and community designations help to implement the region's vision by setting expectations for development density and the character of development throughout the region.

### *Community Designation*

Community designations group jurisdictions with similar characteristics for the application of regional policies. The Council uses community designations to guide regional growth and development; establish land use expectations including overall development densities and patterns; and outline the respective roles of the Council and individual communities, along with strategies for planning for forecasted growth. If there are discrepancies between the *Thrive MSP 2040* Community Designations Map and the Community Designation map contained herein because of adjustments and refinements that occurred subsequent to the adoption of *Thrive*, communities should follow the specific guidance contained in this System Statement.

*Thrive* identifies Coon Rapids with the community designation of Suburban (Figure 1). Suburban communities experienced continued growth and expansion during the 1980s and early 1990s, and typically have automobile-oriented development patterns at significantly lower densities than in previous eras. Suburban communities are expected to plan for forecasted population and household growth at average densities of at least 5 units per acre for new development and redevelopment. In addition, Suburban communities are expected to target opportunities for more intensive development near regional transit investments at densities and in a manner articulated in the *2040 Transportation Policy Plan*.

Specific strategies for Suburban communities can be found on Coon Rapids' [Community Page](#) in the [Local Planning Handbook](#).

## Forecasts

The Council uses the forecasts developed as part of *Thrive* to plan for regional systems. Communities should base their planning work on these forecasts. Given the nature of long-range forecasts and the planning timeline undertaken by most communities, the Council will maintain on-going dialogue with communities to consider any changes in growth trends or community expectations about growth that may have an impact on regional systems.

The *Thrive* forecasts for population, households, and employment for your community are:

	2010 (actual)	2014 (est.)	2020	2030	2040
Population	61,476	62,435	64,800	68,400	72,100
Households	23,532	23,905	25,500	27,500	29,300
Employment	23,260	25,483	27,100	28,900	30,900

## Housing Policy

The Council adopted the [Housing Policy Plan](#) on December 10, 2014, and amended the plan on July 8, 2015. The purpose of the plan is to provide leadership and guidance on regional housing needs and challenges and to support *Thrive MSP 2040*. The *Housing Policy Plan* provides an integrated policy framework to address housing challenges greater than any one city or county can tackle alone.

Consistent with state statute (Minn. Stat. 473.859, subd. 2(c) and subd. 4), communities must include a housing element and implementation program in their local comprehensive plans that address existing and projected housing needs.

The Council has also determined the regional need for low and moderate income housing for the decade of 2021-2030 (see Part III and Appendix B in the *Housing Policy Plan*).

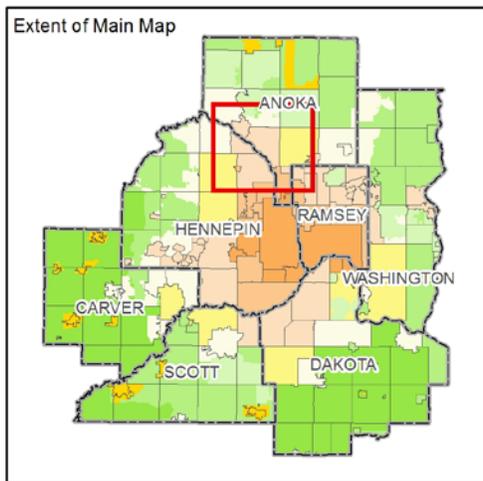
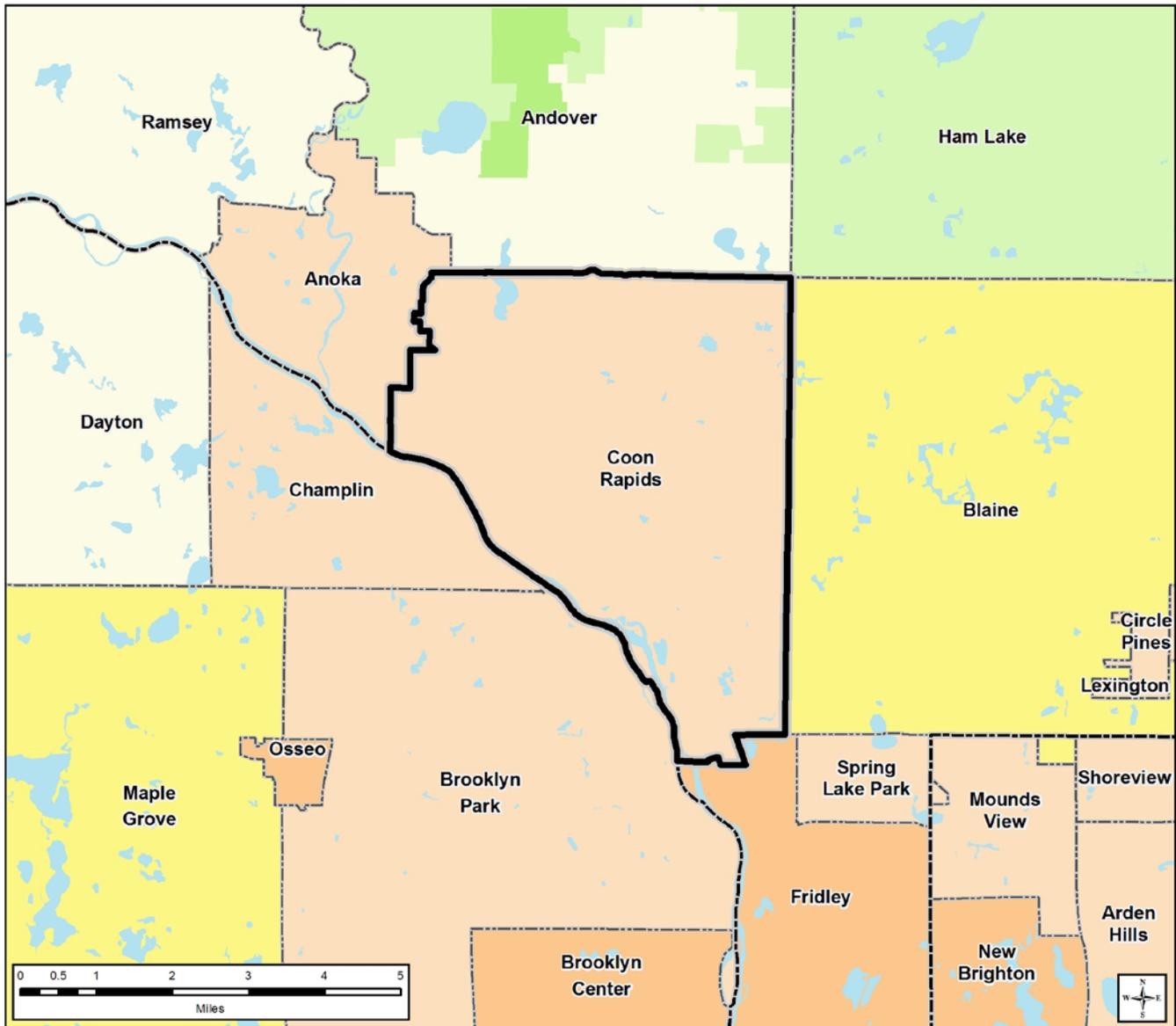
Coon Rapids' share of the region's need for low and moderate income housing is 457 new units affordable to households earning 80% of area median income (AMI) or below. Of these new units, the need is for 240 affordable to households earning at or below 30% of AMI, 53 affordable to households earning 31% to 50% of AMI, and 164 affordable to households earning 51% to 80% of AMI.

### Affordable Housing Need Allocation for Coon Rapids

At or below 30% AMI	240
31 to 50% AMI	53
51 to 80% AMI	164
<b>Total Units</b>	<b>457</b>

Specific requirements for the housing element and housing implementation programs of local comprehensive plans can be found in the [Local Planning Handbook](#).

Figure 1. Coon Rapids Community Designation



**Community Designations**

- Outside Council planning authority
- Agricultural
- Rural Residential
- Diversified Rural
- Rural Center
- Emerging Suburban Edge
- Suburban Edge
- Suburban
- Urban
- Urban Center

- County Boundaries
- City and Township Boundaries
- Lakes and Major Rivers

# TRANSPORTATION SYSTEM STATEMENT

*Coon Rapids*

The *2040 Transportation Policy Plan (TPP)* is the metropolitan system plan for highways, transit, and aviation to which local comprehensive plans must conform. This system statement summarizes significant changes to these three systems, as well as other changes made to the *Transportation Policy Plan* since the last *2030 TPP* was adopted in 2010, and highlights those elements of the system plan that apply specifically to your community. The *TPP* incorporates the policy direction and the new 2040 socio economic forecasts adopted by the Metropolitan Council in the *Thrive MSP 2040*, and extends the planning horizon from 2030 to 2040.

## Federal Requirements

The *TPP* must respond to requirements outlined in state statute, as well as federal law, such as some new requirements included in the federal law known as the Moving Ahead for Progress in the 21<sup>st</sup> Century Act (MAP-21). For instance, metropolitan transportation plans must now be performance based, so the *TPP* now includes goals, objectives, and strategies outlined in chapter 2. In previous versions of the *TPP* the strategies were known as policies; while some are new, the wording of many strategies are similar to the wording of policies in previous plans. Performance measurements for this plan are also discussed in Chapter 12, Federal Requirements.

Federal law requires the long range plan to identify regionally significant transportation investments expected to be made over the next two decades, and to demonstrate that these planned investments can be afforded under the plan's financial assumptions. Both costs and available revenues have changed since the last plan was adopted in 2010, resulting in many changes in the plan. Federal law does allow the plan to provide a vision for how an increased level of transportation revenue might be spent if more resources become available, but the programs or projects identified in this scenario are not considered part of the approved plan.

The *TPP* includes two funding scenarios for the metropolitan highway and transit systems: the "Current Revenue Scenario" and the "Increased Revenue Scenario."

- The **Current Revenue Scenario** represents the fiscally constrained regional transportation plan, which assumes revenues that the region can reasonably expect to be available based on past experience and current laws and allocation formulas.
- The **Increased Revenue Scenario** represents an illustration of what be achieved with a reasonable increase in revenues for transportation.

Under the Metropolitan Land Planning Act, local comprehensive plans are expected to conform to the Current Revenue Scenario, which is the official metropolitan system plan. Potential improvements in the Increased Revenue Scenario can be identified separately in local plans as unfunded proposals. A more detailed description of how to handle the various improvements in this category is included under Other Plan Considerations.

In addition to reviewing this system statement, your community should consult the entire *2040 Transportation Policy Plan* to ensure that your community's local comprehensive plan and plan amendments conform to the metropolitan transportation system plan. Chapter 3, Land Use and Local Planning, has been expanded and all communities should carefully review this chapter. A PDF file of

the entire *2040 Transportation Policy Plan* can be found at the Metropolitan Council's website: [http://www.metrocouncil.org/Transportation/Planning-2/Key-Transportation-Planning-Documents/Transportation-Policy-Plan-\(1\)/The-Adopted-2040-TPP-\(1\).aspx](http://www.metrocouncil.org/Transportation/Planning-2/Key-Transportation-Planning-Documents/Transportation-Policy-Plan-(1)/The-Adopted-2040-TPP-(1).aspx). The format of the plan is slightly different than past *Transportation Policy Plans*. An introductory Overview, Chapter 1: Existing System and Chapter 10: Equity and Environmental Justice have been added to this version of the *TPP*, in addition to the changes noted in the first paragraph. Please note some modifications have been made to the appendices as well.

## **Key Changes in the 2040 Transportation Policy Plan**

Adopted by the Metropolitan Council in January 2015, the revised *2040 Transportation Policy Plan* incorporates the following changes:

### **Metropolitan Highway System - Chapter 5**

The Metropolitan Highway System is made up of principal arterials, shown in Fig 1-1 of the *TPP* and also attached to this system statement. Although no new highways have been added to this system in the *2040 TPP*, the last incomplete segment of this system, TH 610, is now under construction in Maple Grove.

- The *TPP* acknowledges that congestion cannot be eliminated or greatly reduced. The region's mobility efforts will need to focus on managing congestion and working to provide alternatives. The majority of resources available between now and 2040 will be needed for preservation, management and operation of the existing highway system.
- Due to increased costs and decreased revenue expectations, many long-planned major projects to add general purpose highway lanes are not in this fiscally constrained plan. While the preservation, safety, and mobility needs of these corridors are recognized, investments in these corridors will be focused on implementing traffic management strategies, lower cost-high benefit spot mobility improvements, and implementing MnPASS lanes. Some specific projects have been identified in this plan, but funding has primarily been allocated into various investment categories rather than specific projects. The highway projects specifically identified in the Current Revenue Scenario are shown in Figure 5-8 of the *TPP* which is also attached to this system statement.
- Modifications were made to *Appendix D - Functional Classification Criteria*, and *Appendix F – Highway Interchange Requests*. *Appendix C – Project List* is new and contains all of the transit and highway projects that have been identified between 2014 and 2023.

### **Transit System - Chapter 6**

The transit system plan provides an overview of the basic components of transit planning, including demographic factors, transit route and network design factors and urban design factors that support transit usage. Local governments have the primary responsibility for planning transit-supportive land use, through their comprehensive planning, and subdivision and zoning ordinances.

- The *TPP* includes updated Transit Market Areas (shown in *TPP* Figure 6-3, also attached) which reflect 2010 Census information and an updated methodology that better aligns types and levels of transit service to expected demand. These market areas identify the types of transit services that are provided within each area.
- The *TPP* includes limited capital funding for transit expansion and modernization. Opportunities primarily exist through competitive grant programs such as the regional solicitation for US DOT

funding. These opportunities are guided by the strategies in the *TPP* and the various elements of the Transit Investment Plan.

- The *TPP* includes an updated transitway system plan that more clearly articulates which projects can be funded within reasonable revenue expectations through year 2040 (Current Revenue Scenario as shown in *TPP* Figure 6-8, which is also attached). The plan includes five new or expanded METRO lines, three new arterial bus rapid transit lines, and three corridors under study for mode and alignment but identified in the Counties Transit Improvement Board's (CTIB) Phase I Program of Projects. This system was developed in collaboration with CTIB, a major partner in regional transitway expansion.
- The *TPP* does not include operating funding for transit service expansion beyond the existing network of regular route bus, general public dial-a-ride, and Metro Vanpool.
- The Increased Revenue Scenario (shown *TPP* Figure 6-9, which is also attached) illustrates the level of expansion for the bus and support system and transitway system that might be reasonable if additional revenues were made available to accelerate construction of the transitway vision for the region.
- The plan includes updated requirements and considerations for land use planning around the region's transit system. This includes new residential density standards for areas near major regional transit investments and an increased emphasis on proactive land use planning in coordination with the planning of the transit system.

### **Aviation System - Chapter 9**

The Metropolitan Aviation System is comprised of nine airports (shown in Figure 1-9 of the *TPP* and also attached to this system statement) and off-airport navigational aids. There are no new airports or navigational aids that have been added to the system in the *2040 TPP*.

- The *TPP* discusses the regional airport classification system as well as providing an overview of roles and responsibilities in aviation for our regional and national partners. The investment plan includes an overview of funding sources for projects, and an overview of projects proposed for the local airports that will maintain and enhance the regional airport system.
- Modifications were made to *Appendix I – Regional Airspace*, *Appendix J – Metropolitan Airports Commission Capital Investment Review Process*, *Appendix K – Airport Long Term Comprehensive Plans* and *Appendix L – Aviation Land Use Compatibility*.

### **Other Plan Changes**

#### **Regional Bicycle Transportation Network - Chapter 7**

The *2040 TPP* encourages the use of bicycles as a mode of transportation. To that end, the *TPP* establishes for the first time a Regional Bicycle Transportation Network (RBTN). The goal of the RBTN is to establish an integrated seamless network of on-street bikeways and off-road trails that complement each other to most effectively improve conditions for bicycle transportation at the regional level. Cities, counties, and parks agencies are encouraged to plan for and implement future bikeways within and along these designated corridors and alignments to support the RBTN vision.

## Freight - Chapter 8

Most aspects of freight movement are controlled by the private sector, so unlike other sections of the *TPP*, there is not a specific plan adopted for future public sector investment in freight facilities. However, the discussion of the need for a safe and efficient multimodal freight system has been updated and expanded in the *TPP* to recognize challenges and opportunities for freight movement as well as the future direction of freight by mode. It acknowledges the closure of the Minneapolis Upper Harbor in 2015, leaving St Paul and Shakopee as the region’s major barge terminal areas in the future. The plan also acknowledges the increase of trains since 2010 carrying oil from North Dakota on BNSF and CP rail tracks, which is expected to continue into the future. Although railroad trackage in the region was significantly decreased over the last 20 years to “right size” the system after federal deregulation, communities should not expect much additional rail abandonment. Many tracks that appear to be seldom used are owned by the smaller Class III railroads that serve local businesses by providing direct rail connections from manufacturing and warehousing/distribution facilities to the major national railroads. The major Class I railroads are approaching capacity and actually adding tracks in some locations.

## System Plan Considerations Affecting Your Community

Coon Rapids should consult the complete *2040 Transportation Policy Plan* in preparing its local comprehensive plan. In addition, Coon Rapids should consult *Thrive MSP 2040* and the current version of the Metropolitan Council’s *Local Planning Handbook* for specific information needed in its comprehensive plan. Specific system plan considerations affecting Coon Rapids are detailed below.

### Metropolitan Highways

There are three principal arterials located within the Coon Rapids: TH 10, TH 610, and CR 14. The *TPP* does not identify any specific regional mobility improvements on these highways, although maintenance and preservation investments will be made on all highways.

### Transit System

Coon Rapids includes the following Transit Market Areas:

Transit Market Area	Market Area Description and Typical Transit Services
<b>Market Area III</b>	Transit Market Area III has moderate density but tends to have a less traditional street grid that can limit the effectiveness of transit. It is typically Urban with large portions of Suburban and Suburban Edge communities. Transit service in this area is primarily commuter express bus service with some fixed-route local service providing basic coverage. General public dial-a-ride services are available where fixed-route service is not viable.
<b>Market Area IV</b>	Transit Market Area IV has lower concentrations of population and employment and a higher rate of auto ownership. It is primarily composed of Suburban Edge and Emerging Suburban Edge communities. This market can support peak-period express bus services if a sufficient concentration of commuters likely to use transit service is located along a corridor. The low-density development and suburban form of development presents challenges to fixed-route transit. General public dial-a-ride services are appropriate in Market Area IV.

Coon Rapids should identify and map existing transit services and facilities in the local comprehensive plan. Coon Rapids should also work with transit providers serving their community to identify potential future transit service options and facilities that are consistent with the *TPP* and the applicable Transit

Market Areas. Communities can find further maps and guidance for transit planning in the Transportation section of the [Local Planning Handbook](#).

## Transitways

### *Current Revenue Scenario Transitways*

Coon Rapids should acknowledge in your local comprehensive plan the transitway investments planned for your community in the Current Revenue Scenario (*TPP* Figure 6-8). Coon Rapids includes Northstar, which is in operation.

### *Increased Revenue Scenario Transitways*

The *TPP* Increased Revenue Scenario shows additional transitway corridors beyond the scope of the plan's adopted and fiscally constrained Transit Investment Plan (the Current Revenue Scenario). These corridors are listed on page 6.63 of the *TPP*, and *TPP* Figure 6-9, which is attached, shows the complete transitway vision for the region.

If Coon Rapids believes it might be directly impacted by transitways in the Increased Revenue Scenario (for example, because they are participating in transitway corridor studies or feasibility analyses), the transitways may be acknowledged in the Comprehensive Plan. These additional corridors are or will be under study for mode and alignment recommendations, but they are not included in fiscally constrained plan. However, they should be clearly identified as not funded within the currently expected resources for transitways. The Council recognizes the important planning work that goes into a corridor prior to it becoming part of the region's Transit Investment Plan, especially if increased revenues were to become available.

Similar to Current Revenue Scenario Transitways, communities should identify known potential stations along planned transitways and consider guiding land use policies, station area plans, and associated zoning, infrastructure, and implementation tools that support future growth around transit stations. These policies can also influence station siting in initial planning phases of transitway corridors and influence the competitiveness of a transitway for funding. Communities can find further guidance for station area planning in the Transportation section of the [Local Planning Handbook](#) and the *Transit Oriented Development Guide*.

## Aviation

All communities must include an aviation element in the transportation sections of their comprehensive plans. The degree of aviation planning and development considerations that need to be included in the comprehensive plan varies by community. Even those communities not impacted directly by an airport have a responsibility to include airspace protection in their comprehensive plan. The protection element should include potential hazards to air navigation including electronic interference.

Coon Rapids is not in an influence area of a regional airport. Airspace protection should be included in local codes/ordinances to control height of structures.

## Other Plan Considerations

### *Regional Bicycle Transportation Network*

*TPP* Figure 7-1 shows the RBTN as established for the first time in the *2040 TPP*. The network consists of a series of prioritized Tier 1 and Tier 2 corridors and dedicated alignments (routes). The process used to develop the RBTN, as well as the general principles and analysis factors used in its development, can be found in the Bicycle and Pedestrian Chapter of the *TPP*.

The RBTN corridors and alignments make up the “trunk arterials” of the overall system of bikeways that connect to regional employment and activity centers. These are not intended to be the only bicycle facilities in the region, and local units should also consider planning for any additional bike facilities desired by their communities. RBTN corridors are shown where more specific alignments within those corridors have not yet been designated, so local governments are encouraged to use their comprehensive planning process to identify suitable alignments within the RBTN corridors for future incorporation into the *TPP*.

In addition, agencies should plan their local on and off-road bikeway networks to connect to the designated Tier 1 and Tier 2 alignments, as well as any new network alignments within RBTN corridors to be proposed in local comprehensive plans. Bikeway projects that complete segments of, or connect to, the RBTN are given priority for federal transportation funds through the Transportation Advisory Board’s biannual regional solicitation.

Figure 7-1 shows that your community currently has one or more RBTN corridors and alignments within its jurisdiction. The Council encourages local governments to incorporate the RBTN map within their local bicycle plan maps to show how the local and regional systems are planned to work together. An on-line interactive RBTN map, which allows communities to view the RBTN links in their community at a much more detailed scale than Figure 7-1, can be found in the Transportation section of the [Local Planning Handbook](#). The handbook also includes best practices, references, and guidance for all local bicycle planning.

### *A Minor System / Functional Classification*

The *TPP* has always recognized the A minor arterial system as an important supplement to the regional highway system, and the Transportation Advisory Board (TAB) continues to maintain the official regional map of these roads. The *2040 TPP* does include an updated functional classification map (Fig. 1-2 in Chapter 1) and a modified *Appendix D - Functional Classification Criteria*. Communities should consult the Local Plan Handbook for more information on functional classification, how to reflect the A minor arterial system in their plan, and how to request functional classification changes if necessary.

### *Freight*

The Council encourages all local governments to plan for freight movement in their communities. Trucks are the major mode of freight movement in the region and across the nation to distribute consumer goods as well as move manufactured goods and commodities, and they operate in every community.

Communities with special freight facilities shown on *TPP* Figure 8-1, Metropolitan Freight System, (attached) should also include those additional modes and facilities in their local plan, and plan for compatible adjacent land uses.

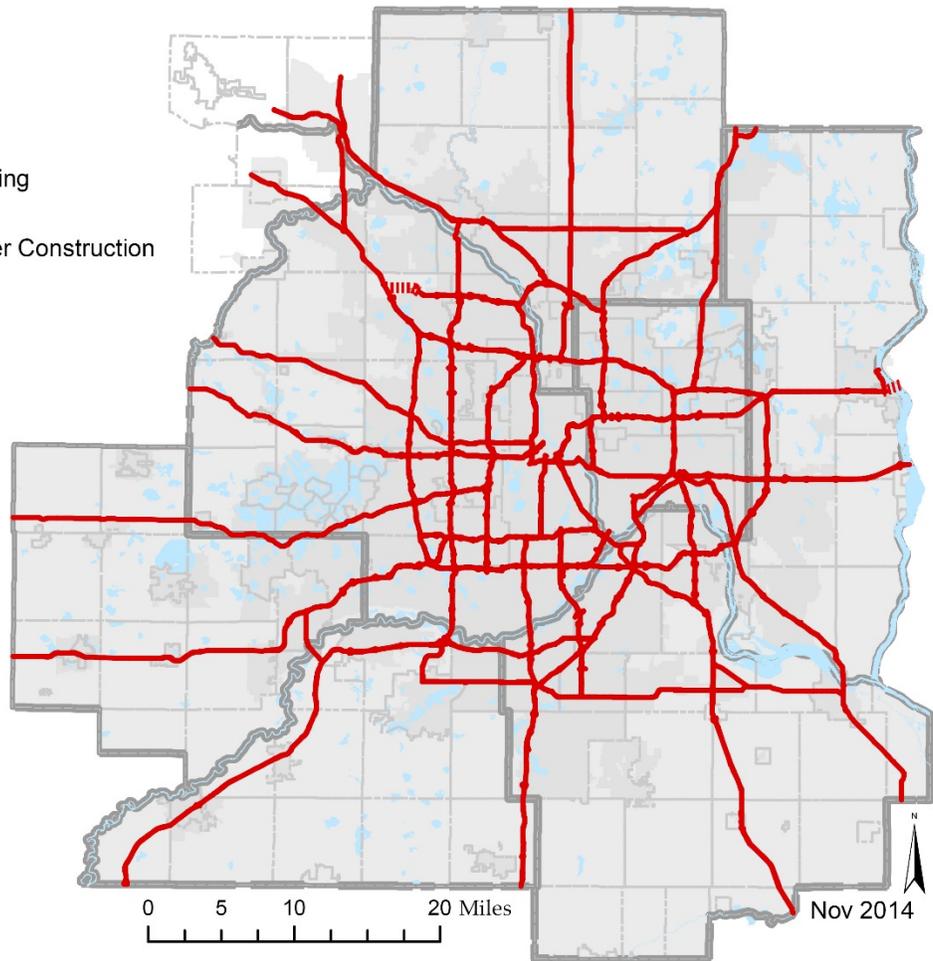
Figure 1-1 of the TPP

### Principal Arterials

-  Principal Arterial - Existing
-  Principal Arterial - Under Construction

### Reference Items

-  Lakes and Rivers
-  City Boundary
-  County Boundary
-  MUSA 2040 MPO Area



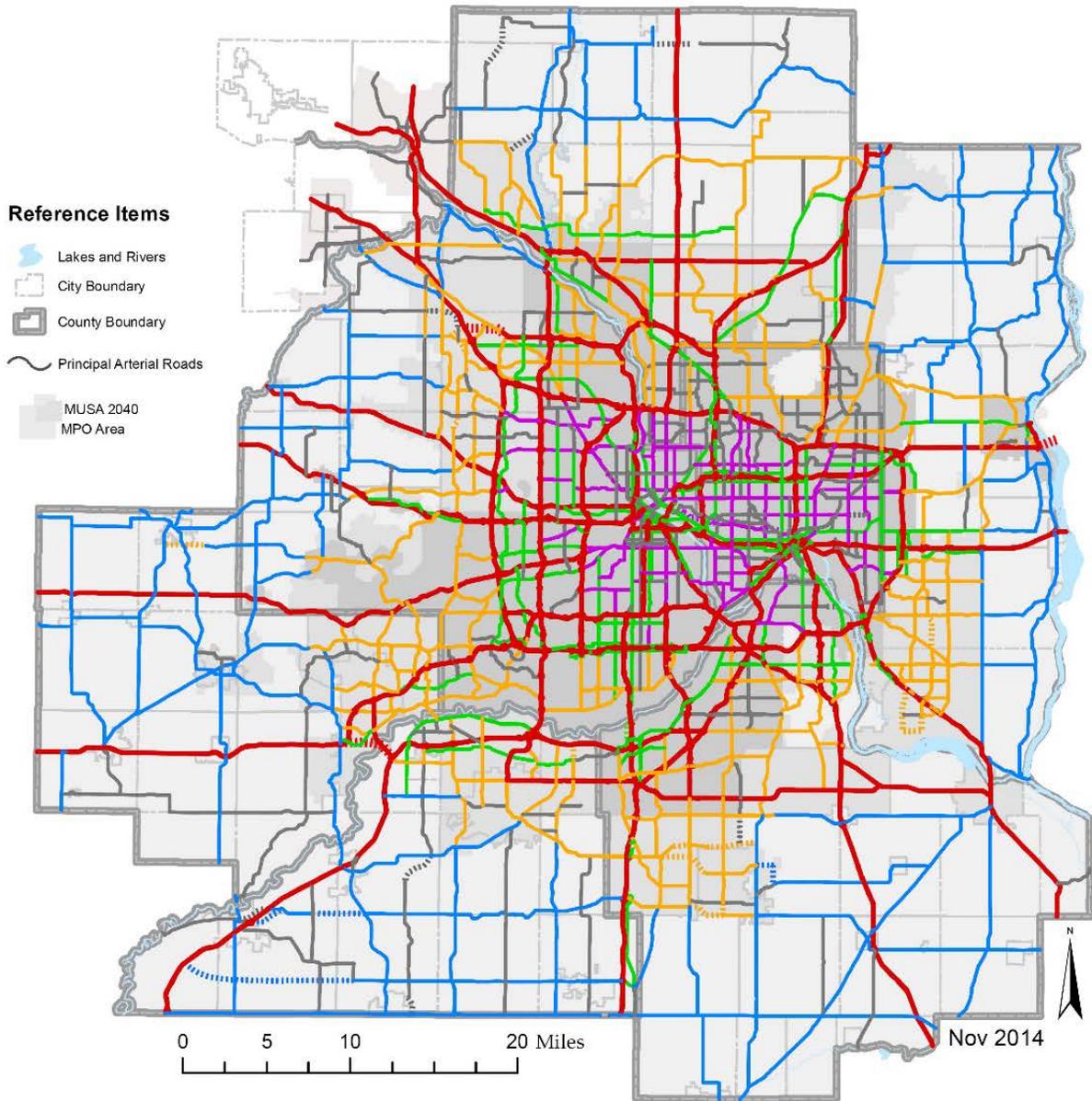
2040 TRANSPORTATION POLICY PLAN | METROPOLITAN COUNCIL

Figure 1-1



Figure 1-2 of the TPP

## Functional Class Roads



### Existing

- Principal Arterial
- A-Minor Augmentor
- A-Minor Reliever
- A-Minor Expander
- A-Minor Connector
- Other Minor Arterial

### Planned

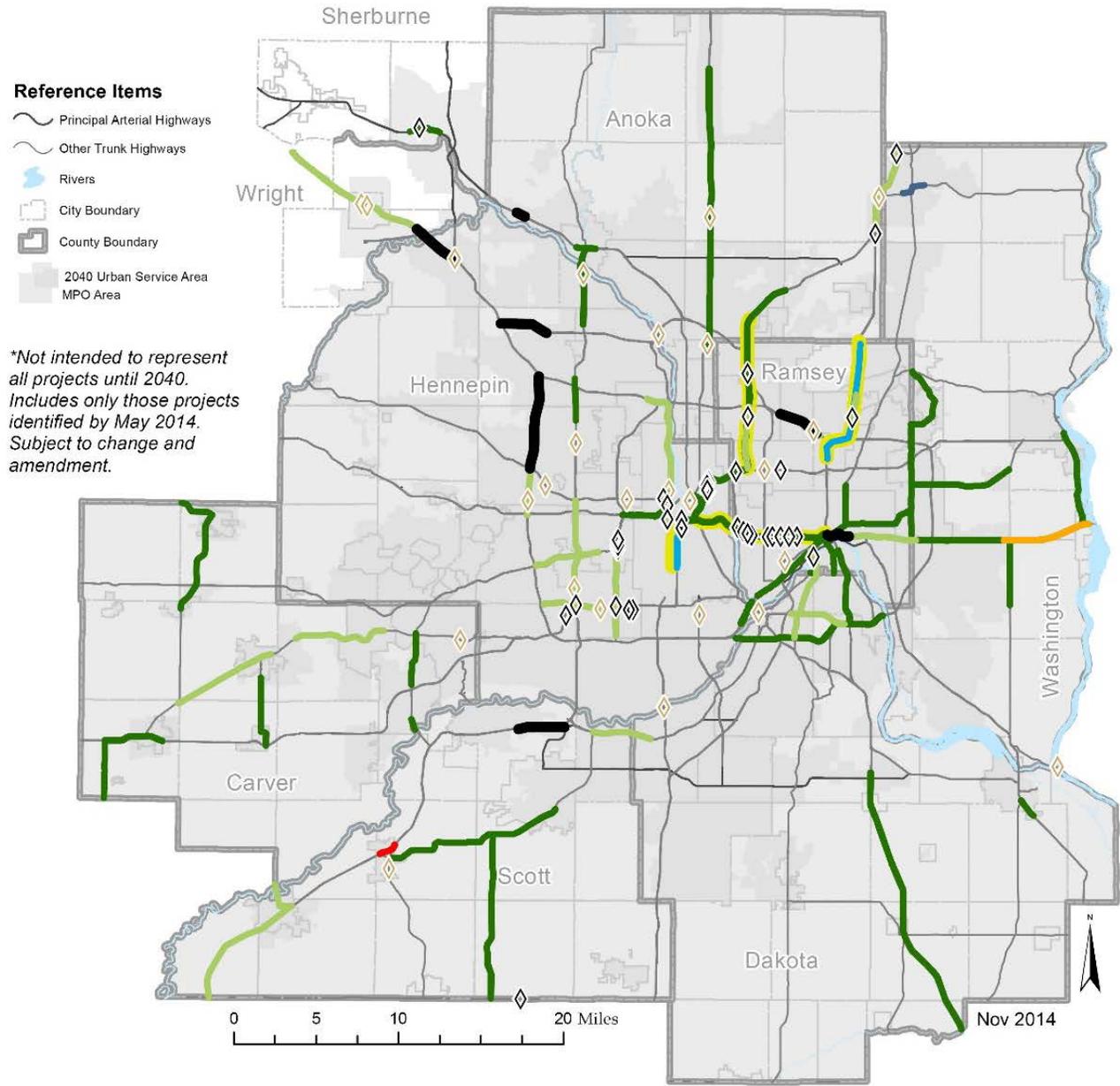
- Principal Arterial
- A-Minor Augmentor
- A-Minor Reliever
- A-Minor Expander
- A-Minor Connector
- Other Minor Arterial

### Thrive Planning Areas

- Urban Core & Urban & Suburban
- Suburban Edge & Emerging Suburban Edge
- Rural Service Areas
- MPO Area outside the Seven County Area

Figure 5-8 of the TPP

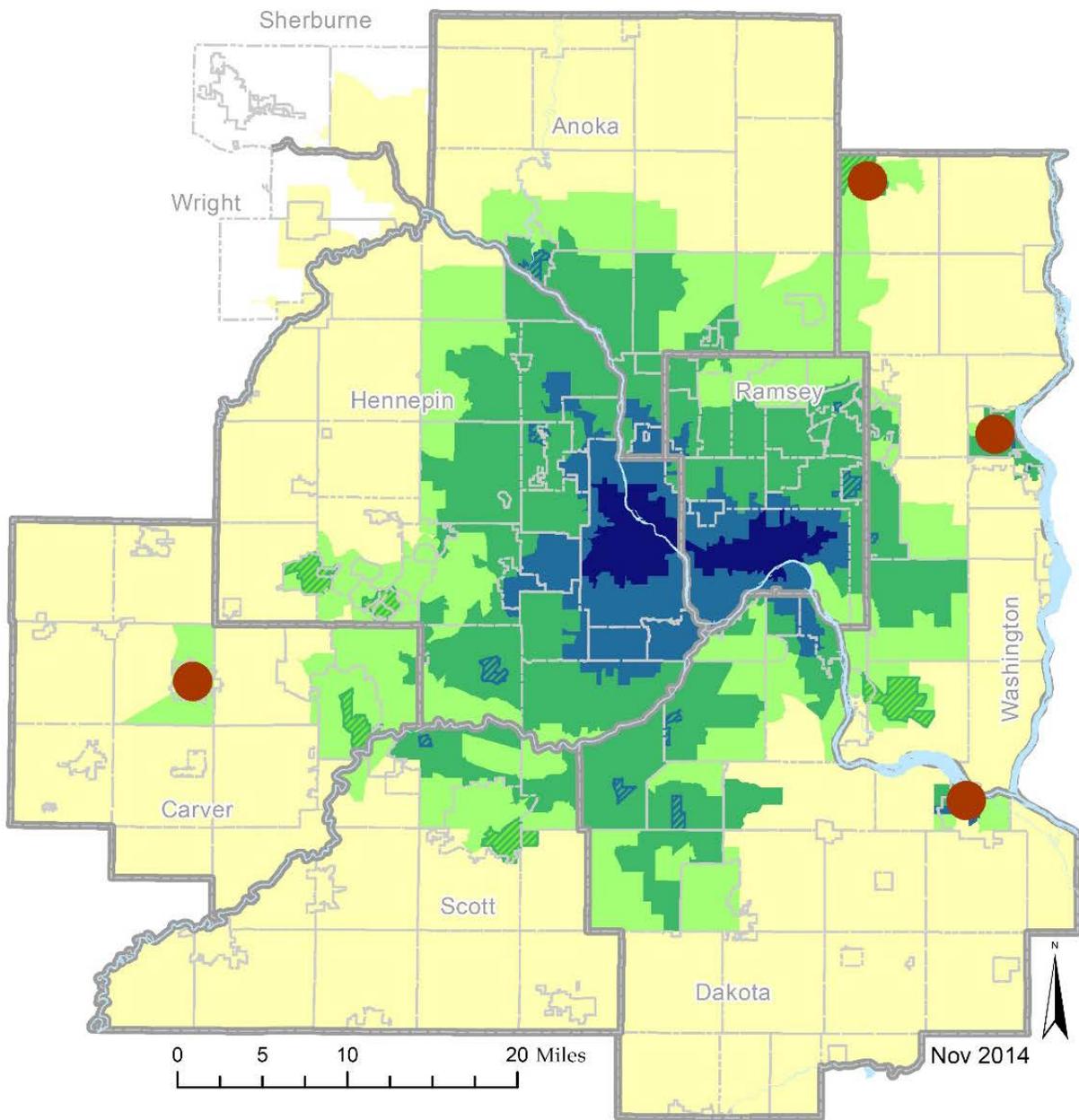
### Identified Projects\* in Highway Current Revenue Scenario



- 2015-2018 TIP Bridges
- Strategic Capacity
- 2019 - 2024 Pavement Projects
- 2019 - 2024 Bridges
- Roadside Infrastructure
- 2015 - 2018 Pavement / MnPass
- Roadside Infrastructure / Safety
- 2015-2018 TIP Pavement
- 2015 - 2018 Pavement / Safety
- Tier 1 MnPASS Expansion

Figure 6-3 of the TPP

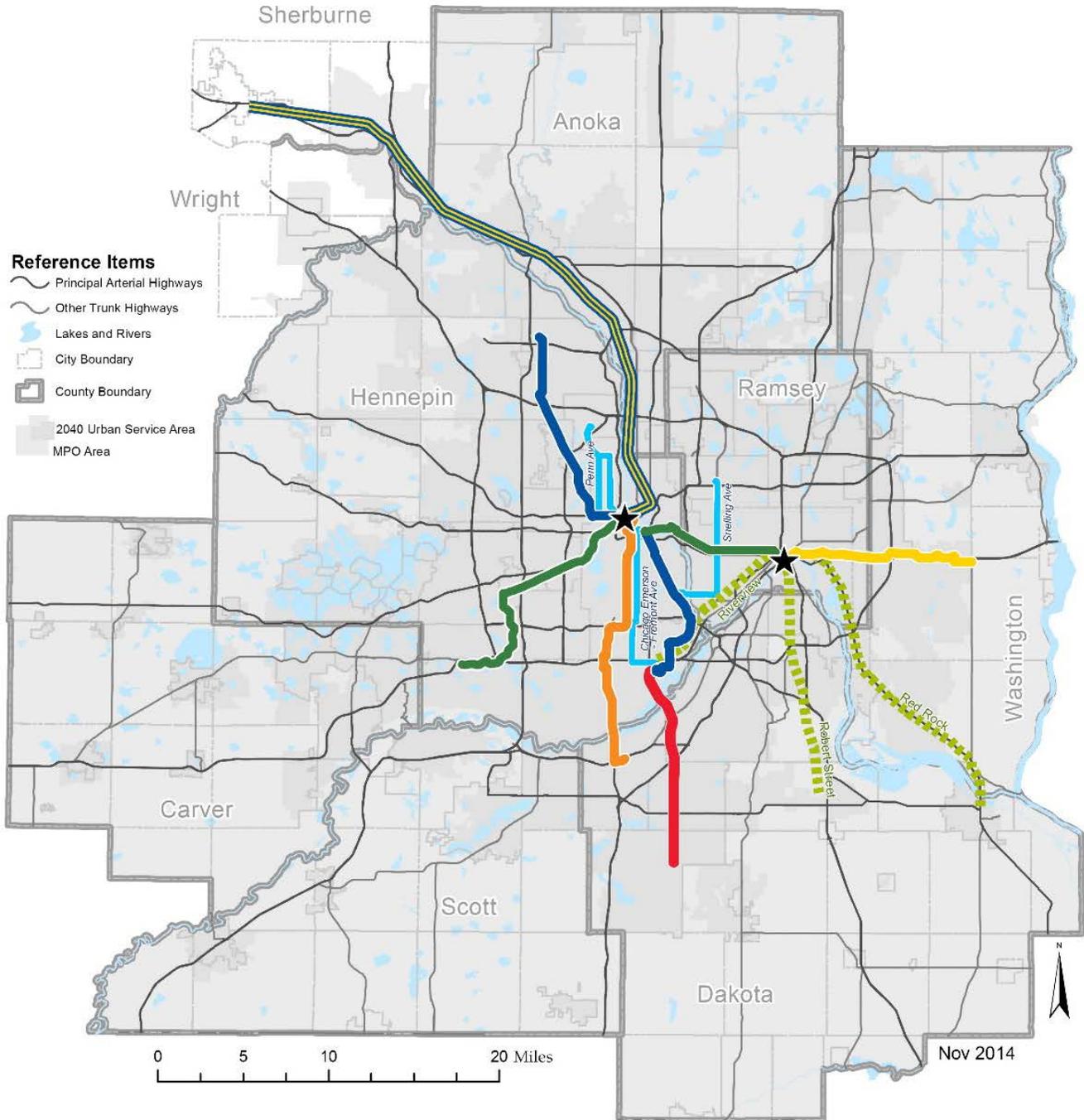
### Transit Market Areas



- Market Area I
- Market Area II
- Emerging Market Area II
- Market Area III
- Emerging Market Area III
- Market Area IV
- Market Area V
- Freestanding Town Center

Figure 6-8 of the TPP

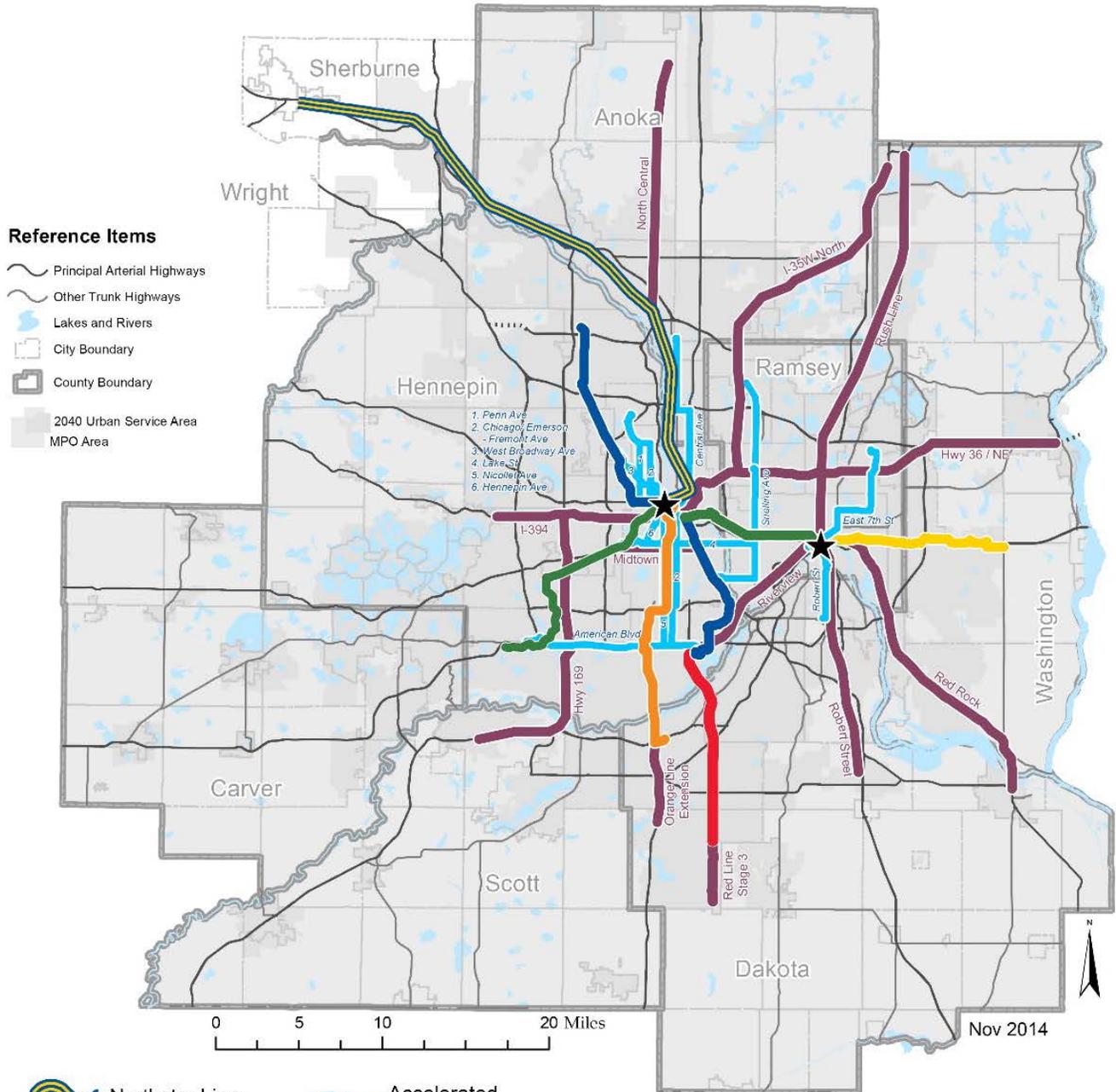
# Current Revenue Scenario Transitways and CTIB Phase I Program of Projects



- |  |   |   |
|--|---|---|
|  Northstar Line |  Red Line    |  Arterial BRT  |
|  Blue Line      |  Orange Line |  CTIB Phase I Program of Projects under study mode and alignment not yet specified |
|  Green Line     |  Gold Line   |  Regional Multimodal Hub   |

Figure 6-9 of the TPP

# Increased Revenue Scenario Transitways Building an Accelerated Transitway Vision

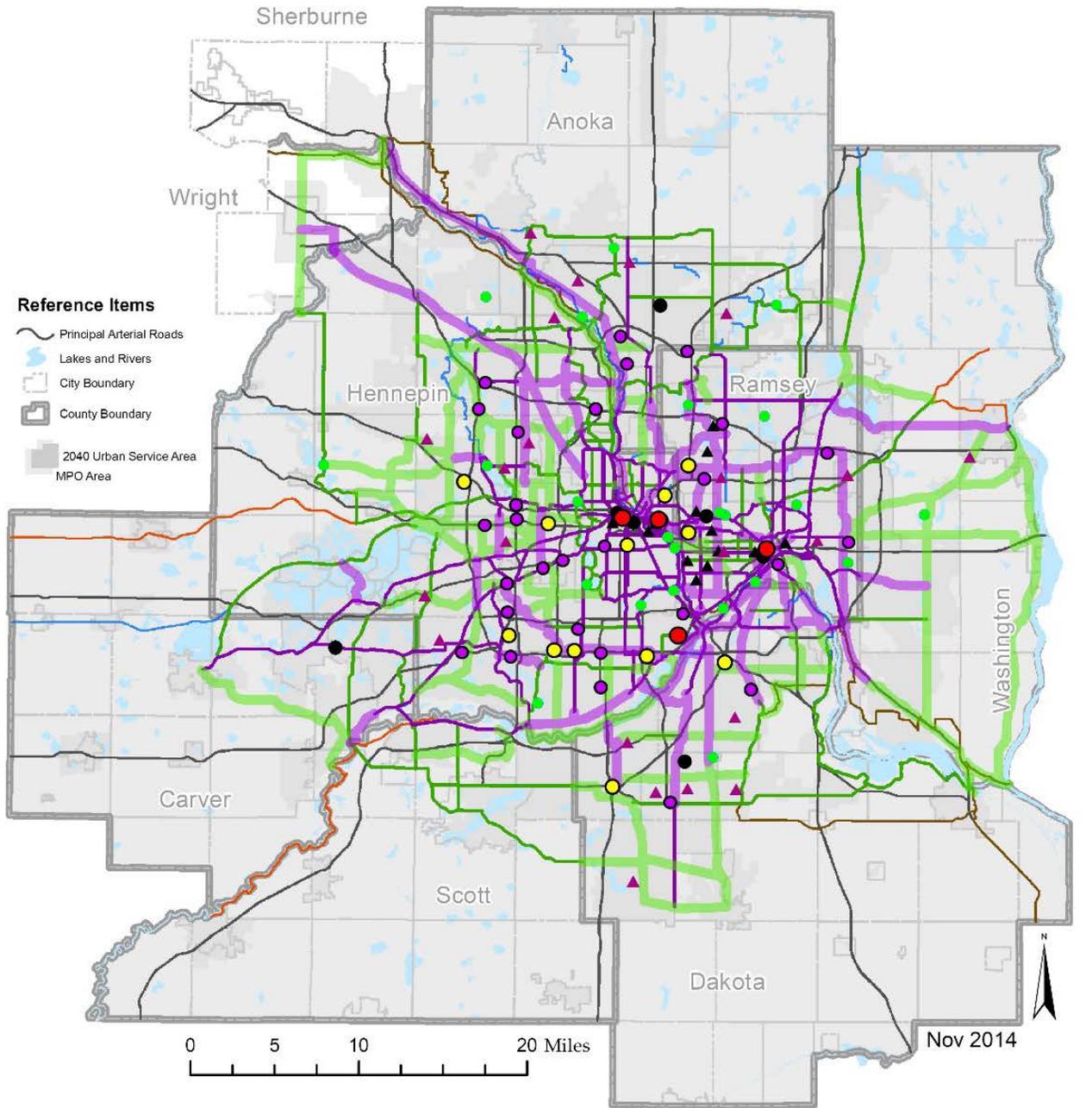


- Northstar Line
- Blue Line
- Green Line
- Red Line
- Orange Line
- Gold Line
- Accelerated Arterial BRT
- Accelerated Transitways under study mode and alignment not yet specified
- Regional Multimodal Hub

**Increased Revenue Scenario would also include at least 1% average annual bus expansion.**

Figure 7-1 of the TPP

## Regional Bicycle Transportation Network Vision



### RBTN Alignments

- Tier 1 Alignments
- Tier 2 Alignments

### RBTN Corridors (Alignments Undefined)

- Tier 1 Priority Regional Bicycle Transportation Corridor
- Tier 2 Regional Bicycle Transportation Corridors

### Regional Destinations

- Metropolitan Job Centers
- Regional Job Centers
- Subregional Job Centers
- Large High Schools
- Colleges & Universities
- Highly Visited Regional Parks
- Major Sport & Entertainment Centers

### Other Trail Systems

- Regional Trails (Regional Parks Policy Plan)
- Mississippi River Trail (US Route 45)
- State Trails (DNR)

Figure 8-1 of the TPP

## Metropolitan Freight System

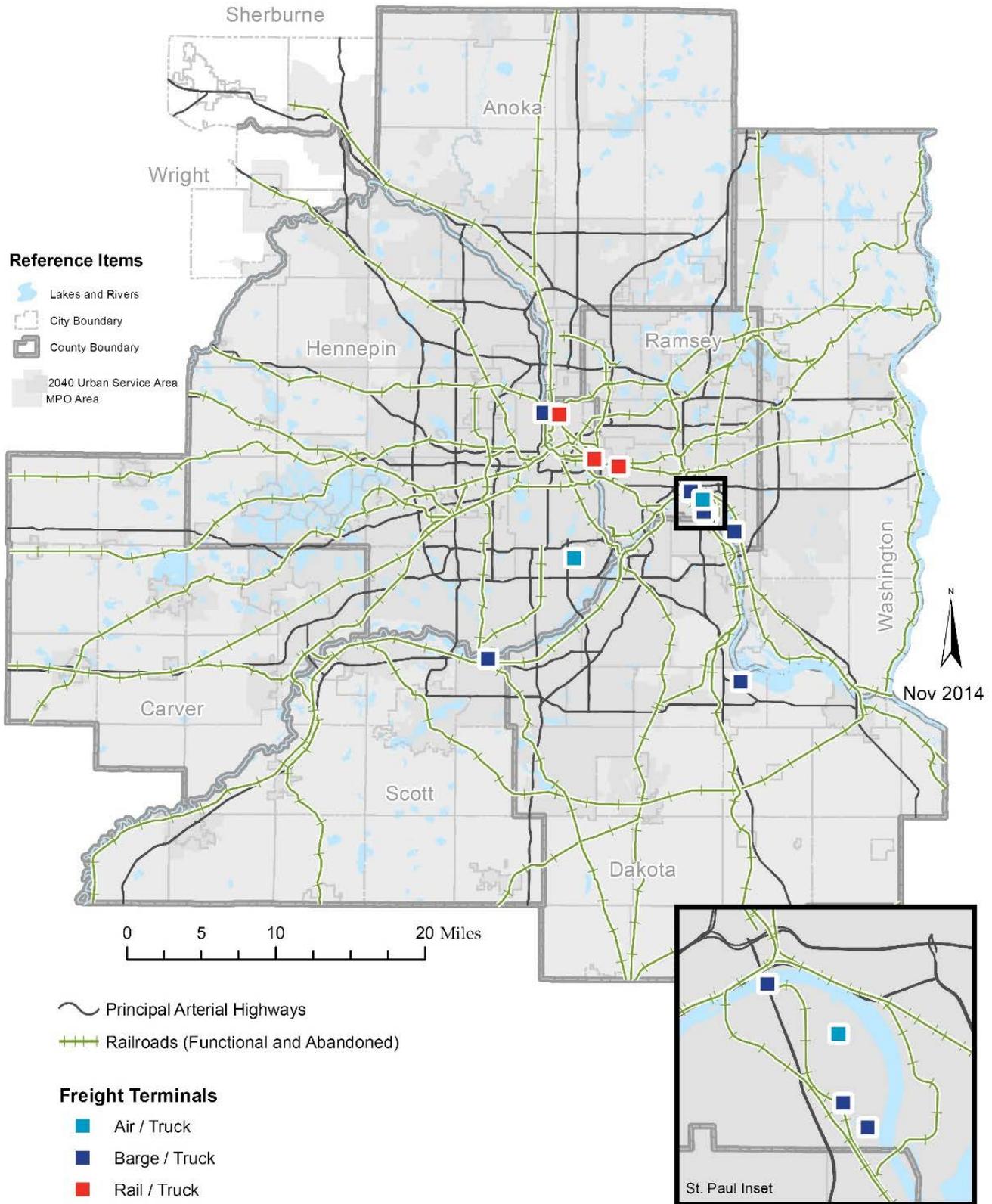
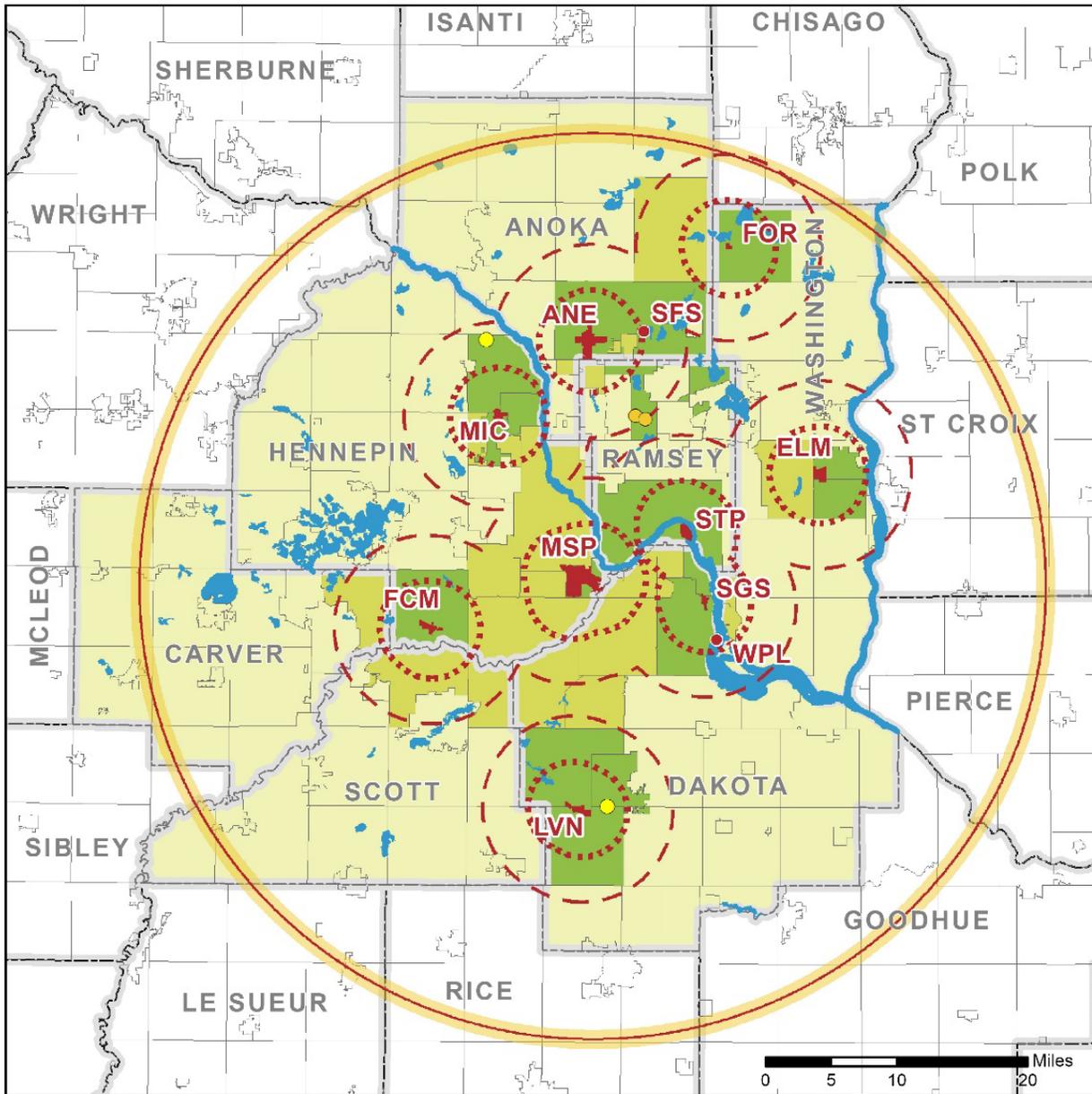
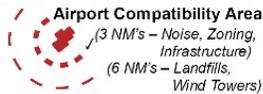


Figure 9-1 of the TPP

### Airport Service Areas



**Public Owned Public Use Airport**



**MSP** Minneapolis - St. Paul International Airport (Wold-Chamberlain Field)

**STP** St. Paul Downtown Airport (Holman Field)

**ANE** Anoka County - Blaine Airport (Janes Field)

**FCM** Flying Cloud Airport

**MIC** Crystal Airport

**SGS** South St. Paul Airport (Fleming Field)

**ELM** Lake Elmo Airport

**LVN** Airlake Airport

**FOR** Forest Lake Airport

**Privately Owned Public Use Airport**

**SFS** Surf-Side Seaplane Base (Rice Lake)

**WPL** Wipline Seaplane Base (Miss. River)

Minneapolis Class-B Airspace Boundary

Permitted Seaplane Surface Waters (within 7 County Area only)

VOR Protection Zone

Tall Tower Areas

Aviation Facility Located in Community

Community Directly Affected by Facility(s)

General Airspace Notification/Protection

# WATER RESOURCE REQUIREMENTS/ WASTEWATER SYSTEM STATEMENT

*City of Coon Rapids*

The *2040 Water Resources Policy Plan* includes policies and strategies to achieve the following goal:

*To protect, conserve, and utilize the region's groundwater and surface water in ways that protect public health, support economical growth and development, maintain habitat and ecosystem health, and provide for recreational opportunities, which are essential to our region's quality of life.*

The Policy Plan takes an integrated approach to water supply, water quality, and wastewater issues. This approach moves beyond managing wastewater and stormwater only to meet regulatory requirements by viewing wastewater and stormwater as resources, with the goal of protecting the quantity and quality of water our region needs now and for future generations.

The Policy Plan includes policies and strategies to:

- Maximize regional benefits from regional investments in the areas of wastewater, water supply and surface water.
- Pursue reuse of wastewater and stormwater to offset demands on groundwater supplies.
- Promote greater collaboration, financial support, and technical support in working with partners to address wastewater, water quality, water quantity and water supply issues.
- Implement environmental stewardship in operating the regional wastewater system by reusing wastewater, reducing energy use and air pollutant emissions, and reducing, reusing, and recycling solid waste.

## **Key Concepts in the 2040 Water Resources Policy Plan**

Adopted by the Metropolitan Council in May 2015, the *2040 Water Resources Policy Plan* is the metropolitan system plan for metropolitan wastewater services with which local comprehensive plans must conform. The Policy Plan incorporates the following changes:

- Centers on and around an integrated approach to water supply, wastewater, and surface water planning.
- Promotes the investigation of the issues and challenges in furthering our work in water conservation, wastewater and stormwater reuse, and low impact development practices in order to promote a more sustainable region.
- Promotes the concept of sustainable water resources where, through collaboration and cooperation, the region will take steps to manage its water resources in a sustainable way aimed at:
  - Providing an adequate water supply for the region
  - Promoting and implementing best management practices that protect the quality and quantity of our resources
  - Providing efficient and cost effective wastewater services to the region
  - Efficiently addressing nonpoint and point sources pollution issues and solutions, and,
  - Assessing and monitoring lakes, rivers, and streams so that we can adequately manage, protect, and restore our valued resources.
- Continues the Council's position that communities that permit the construction and operation of subsurface sewage treatment systems and other private wastewater treatment systems are

responsible for ensuring that these systems are installed, maintained, managed and regulated consistent with Minnesota Rules Chapter 7080-7083.

- Includes requirements in Appendix C for comprehensive sewer plans, local water plans, and local water supply plans.
- Establishes inflow and infiltration goals for all communities served by the regional wastewater system and requires all communities to include their inflow and infiltration mitigation programs in their comprehensive sewer plan.
- Works with the State to attempt to (1) make funds available for inflow and infiltration mitigation, and (2) promote statutes, rules, and regulations to encourage I/I mitigation.

Coon Rapids should consult the complete Policy Plan in preparing its local comprehensive plan. In addition, Coon Rapids should consult *Thrive MSP 2040* and the *Local Planning Handbook* for specific information needed in its comprehensive plan.

## System Plan Considerations Affecting Your Community

### *Metropolitan Sewer Service*

Under state law (Minn. Stat. 473.513) local governments are required to submit both a wastewater plan element to their comprehensive plan as well as a comprehensive sewer plan describing service needs from the Council. Specific requirements for the sewer element of your comprehensive plan can be found in the Water Resources section of the *Local Planning Handbook*.

### Forecasts

The forecasts of population, households, employment, and wastewater flows for Coon Rapids as contained in the adopted *2040 Water Resources Policy Plan* can be found at: <http://www.metrocouncil.org/Wastewater-Water/Planning/2040-Water-Resources-Policy-Plan.aspx> and on your Community Page in the *Local Planning Handbook*. These forecasts are for sewered development. The sewered housing forecasts were estimated using SAC data, annual city reports, current trends, existing and future local wastewater service areas and other information relating to your community. The wastewater flows are based on historical wastewater flow data, future projected wastewater generation rates, and the projected sewered population and employment data.

The Council will use these growth and wastewater flow forecasts to plan future interceptor and treatment works improvements needed to serve your community. The Council will not design future interceptor improvements or treatment facilities to handle peak hourly flows in excess of the allowable rate for your community. Coon Rapids, through its comprehensive planning process, must decide the location and staging of development, and then plan and design its local wastewater collection system to serve this development. The Council will use its judgment as to where to assign growth within your community to determine regional system capacity adequacy. If Coon Rapids wishes to identify specific areas within the community to concentrate its growth, it should do so within its Comprehensive Sewer Plan.

You should also note that urban development at overall densities that are substantially lower than those identified for your community in the Community Designation Section of this Systems Statement will also be analyzed by the Council for their potential adverse effects on the cost of providing metropolitan sewer service.

### Description of the Metropolitan Disposal System Serving Your Community

Figure 1 shows the location of the Metropolitan Disposal System (MDS) serving your community. Wastewater flow from Coon Rapids is treated at the Metropolitan WWTP.

## **Description of the Regional Inflow/Infiltration (I/I) Program**

The *2040 Water Resources Policy Plan* states that the Council will establish I/I goals for all communities discharging wastewater to the MDS. Communities that have excessive I/I in their sanitary sewer systems will be required to eliminate excessive I/I. The Council will continue the implementation of its on-going I/I reduction program. Communities identified through the program as needing to eliminate excessive I/I will be required to submit a work plan that details work activities to identify and eliminate sources of I/I. The Council can limit increases in service within those communities having excess I/I that do not demonstrate progress in reducing their excess I/I. The Council will meet with the community and discuss this alternative before it is implemented.

It is required that those communities that have been identified as contributors of excessive I/I, and that have not already addressed private property sources, do so as part of their I/I program. Significant work has been accomplished on the public infrastructure portion of the wastewater system. The Council will pursue making funds available through the State for I/I mitigation, and promote statutes, rules and regulations to encourage I/I mitigation.

## ***Management of Subsurface Sewage Treatment Systems (SSTS) and Private Systems***

The Metropolitan Land Planning Act requires the sewer element of the local comprehensive plan to describe the standards and conditions under which the installation of subsurface sewage treatment systems and other private wastewater treatment systems will be permitted and to the extent practicable, the areas not suitable for public or private systems.

The appropriate density for development with subsurface sewage treatment systems depends on the suitability of the soils to treat wastewater and whether space is available for a primary and back up drainfield. It is the Council's position that all municipalities and counties allowing subsurface sewage treatment systems should incorporate current MPCA regulations (Minn. Rules Chapter 7080-7083) as part of a program for managing subsurface sewage treatment systems in the sewer element of their local comprehensive plan and implement the standards in issuing permits.

Coon Rapids should adopt a management program consistent with state rules. An overview of Coon Rapids's management program must be included in the community's local comprehensive plan update. If adequate information on the management program is not included; the comprehensive plan will be found incomplete for review until the required information is provided to the Council. Specific requirements for the local comprehensive plan can be found in the [Local Planning Handbook](#).

Small private treatment plants are located throughout the Metropolitan Area serving such developments as individual industries, mobile home parks, and other urban type uses. The Council's position is that such private wastewater treatment plants should be permitted only if they are in areas not programmed for metropolitan sewer service in the future and they are provided for in a community's comprehensive plan that the Council has approved. Furthermore, the community is responsible for permitting all community or cluster wastewater treatment systems consistent with Minnesota Rules Chapter 7080-7083 and MPCA standards. The Council will not provide financial support to assist communities if these systems fail.

Coon Rapids should include in the sewer element of its local comprehensive plan the conditions under which private treatment plants or municipal treatments would be allowed, and include appropriate management techniques sufficiently detailed to ensure that the facilities conform to permit conditions. Coon Rapids is responsible for ensuring that permit conditions for private treatment plants are met and financial resources to manage these facilities are available.

## Surface Water Management

In 1995, Minnesota Statutes Section 473.859, subd. 2 was amended to make the local water plan (often referred to as local surface water management plans) required by section 103B.235 a part of the land use plan of the local comprehensive plan. Minnesota Rules Chapter 8410, updated in July of 2015, includes the requirements for local water management plans. The main change that you need to be aware of is that all communities in the metropolitan area must update their local water plan between January 1, 2017 and December 31, 2018. This means that Coon Rapids must update its local water plan as part of the comprehensive plan update. The community's updated local water plan should be submitted to the Council for its review concurrent with the review by the Watershed Management Organization(s) within whose watershed(s) the community is located. **Failure to have an updated local water plan will result in the comprehensive plan being found incomplete for review until the required plan is provided to the Council.**

Local water plans must meet the requirements for local water plans in Minnesota Statutes, section 103B.235 and Minnesota Rules Chapter 8410. In general, local surface water plans need to include a summary of the priorities and problems in the community; structural, nonstructural and programmatic actions to take to address the priorities and problems; and clearly identified funding mechanisms to fix the problems.

More detailed guidance for the local water plans can be found in Appendix C of the Council's *2040 Water Resources Policy Plan* and in the Council's current *Local Planning Handbook*.

In addition, the Council has also updated its priority lake list that was first developed in the 1980s as part of the *Water Resources Policy Plan* update. Figure 2 shows the priority lakes for Coon Rapids. The Council uses the priority lake list to focus its limited resources. The list is also used in the environmental review process. Where a proposed development may impact a priority lake, the project proposer must complete a nutrient budget analysis for the lake as part of the environmental review process.

Also included on Figure 2 is the watershed organization(s) that Coon Rapids is part of and a list of impaired waters in the community for use in development of your local water plans.

## Other Plan Considerations

### Water Supply

Local comprehensive plans also address water supply (Minn. Stat., Sec. 473.859). For communities in the metropolitan area with municipal water supply systems, this local comprehensive plan requirement is met by completing the local water supply plan template, which was jointly developed by the Metropolitan Council and the Minnesota Department of Natural Resource (DNR).

#### FOR COMMUNITIES WHO OWN/OPERATE A PUBLIC WATER SUPPLY SYSTEM:

Because your community owns/operates a municipal community public water supply system (PWS), the local water supply plan must be updated as part of the local comprehensive plan (Minn. Stat., Sec. 103G.291).

**The updated local water supply plan should include information about your community along with information about any neighboring communities served by your system.**

You should update your local water supply plan upon notification by DNR. Local water supply plan due dates will be staggered between January 1, 2017 and December 31, 2018. Your updated local water

supply plan should be submitted to the DNR. DNR will share the plan with the Council, and it will be reviewed concurrently by both agencies. This schedule allows the local water supply plans to be completed and included in the local comprehensive plan.

**Failure to have an updated local water plan will result in the comprehensive plan being found incomplete for review until the required plan is provided to the Council.**

The water supply plan template fulfills multiple statutory obligations including:

- Minn. Stat., Sec. 103G.291 to complete a water supply plan including demand reduction
- Minn. Stat., Sec. 473.859 to address water supply in local comprehensive plans
- Minn. Administrative Rules 4720.5280 to address contingency planning for water supply interruption

The plan must be officially adopted by your community, and if applicable the utility board, as part of the local comprehensive plan.

At a minimum, the updated local water supply plan must use the joint DNR and Metropolitan Council template and include water demand projections that are consistent with the community's population forecast provided in the introductory section of this system statement. Potential water supply issues should be acknowledged, monitoring and conservation programs should be developed, and approaches to resolve any issues should be identified.

Guidance and information for water supply planning can be found in the Appendix C of the *2040 Water Resources Policy Plan*, the *Local Planning Handbook*, and the Council's *Master Water Supply Plan*.

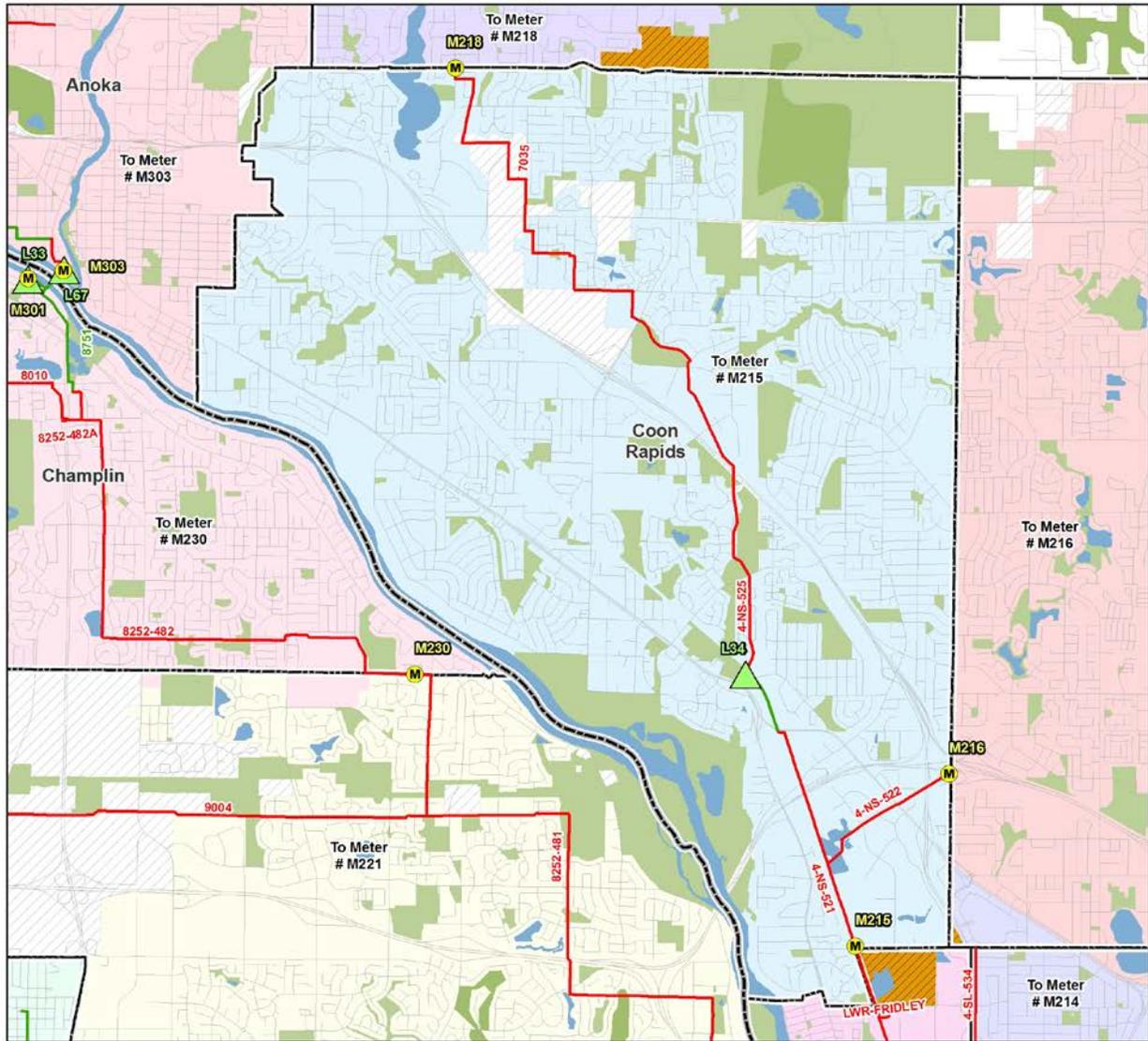
The Council's *Master Water Supply Plan* provides communities in the region with planning assistance for water supply in a way that:

- Recognizes local control and responsibility for owning, maintaining and operating water systems
- Is developed in cooperation and consultation with municipal water suppliers, regional stakeholders and state agencies
- Protects critical habitat and water resources over the long term
- Meets regional needs for a reliable, secure water supply
- Highlights the benefits of integrated planning for stormwater, wastewater and water supply
- Emphasizes and supports conservation and inter-jurisdictional cooperation
- Provides clear guidance by identifying key challenges/issues/considerations in the region and available approaches without dictating solutions

Figures 3-5 illustrate some water supply considerations that the community may consider as they develop their local water supply plans, such as: aquifer water levels, groundwater and surface water interactions, areas where aquifer tests or monitoring may be needed to reduce uncertainty, regulatory and management areas, and emergency interconnections.

Figure 1. MCES Sanitary Sewer Meter Service Areas

### City of Coon Rapids, Anoka County

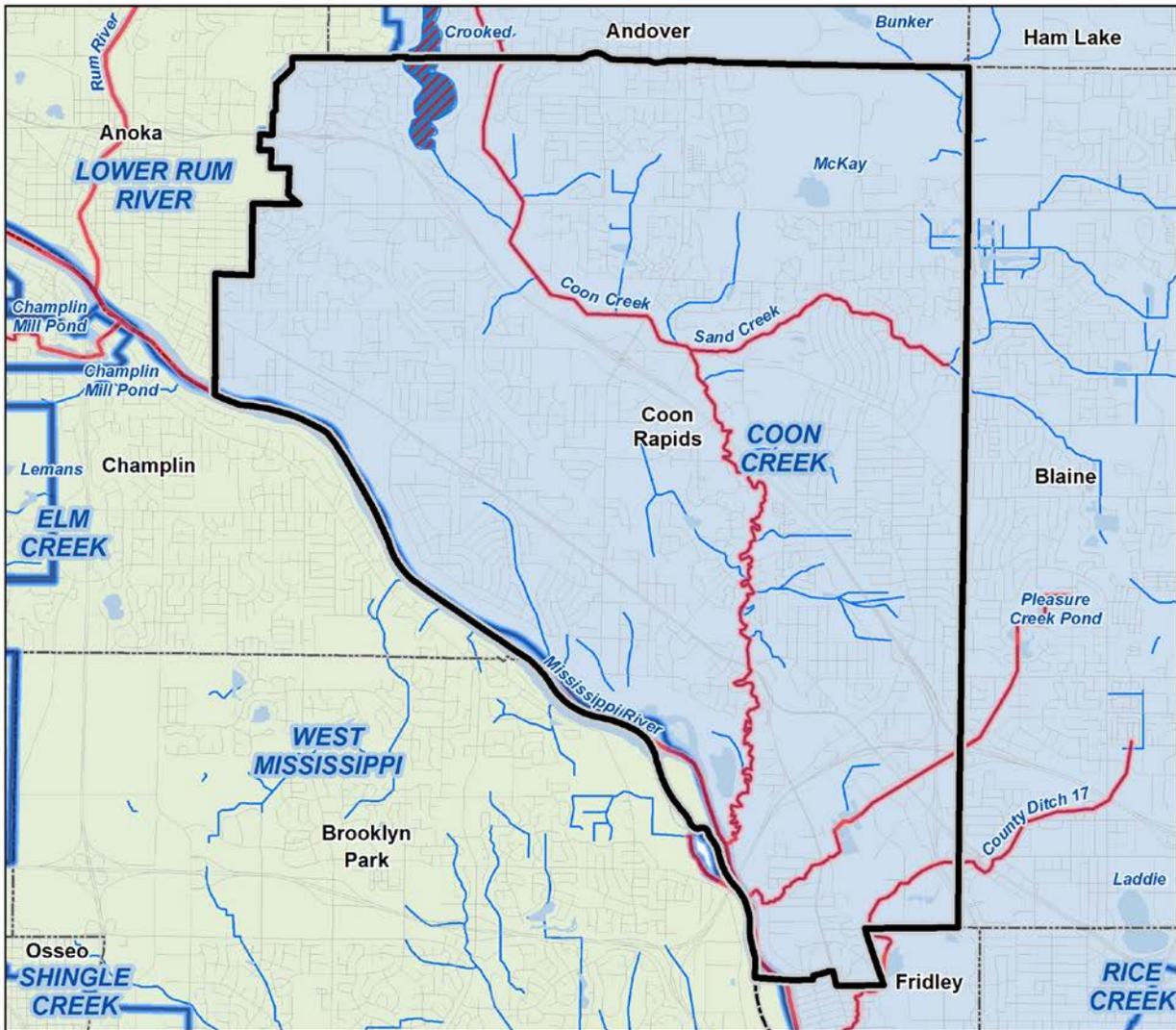


1/2/2015

- |                             |                     |  |
|-----------------------------|---------------------|--|
| <b>Interceptors by Type</b> | — Outfall           | Ⓜ Meters                               |
| — Gravity                   | — Low Head Crossing | ▲ Lift Stations                        |
| — Forcemain                 | — Bypass            | Ⓜ WTP MCES Wastewater Treatment Plants |
| — Siphon                    |                     |  |
- 
- |  |                    |
|--|--------------------|
| <b>Interceptor Meter Service Areas</b> |                    |
| ■ To Meter # 100                       | ■ Areas Not Served |
- 
- |  |                                |                                  |
|--|--------------------------------|----------------------------------|
| ■ Areas of Unmetered Flow into the Community | ▭ County Boundaries            | ■ Park, Recreational or Preserve |
| ■ Rural Center WWTP Service Areas            | ▭ City and Township Boundaries | ■ Golf Course                    |
| ▨ 2040 MUSA                                  | ▭ Lakes and Rivers             |                                  |
|  | ▭ NCompass Street Centerlines  |                                  |

Figure 2. Surface Water Resources

## Coon Rapids, Anoka County



- |  |   |
|--|---|
|  Watershed Management Organization Boundaries |  Impaired Rivers & Streams (2014 Draft MPCA 303(d) List) |
| <b>Watershed Management Organization Type</b>  |  2014 Priority Lakes                                     |
|  County                                       |  County Boundaries                                       |
|  Watershed District                           |  City and Township Boundaries                            |
|  Watershed Management Organization            |  Other Lakes and Major Rivers                            |
|  |  Other Streams   |
|  |  NCompass Street Centerlines                             |

Figure 3. Surface water features and interaction with the regional groundwater system, and state-protected surface water features

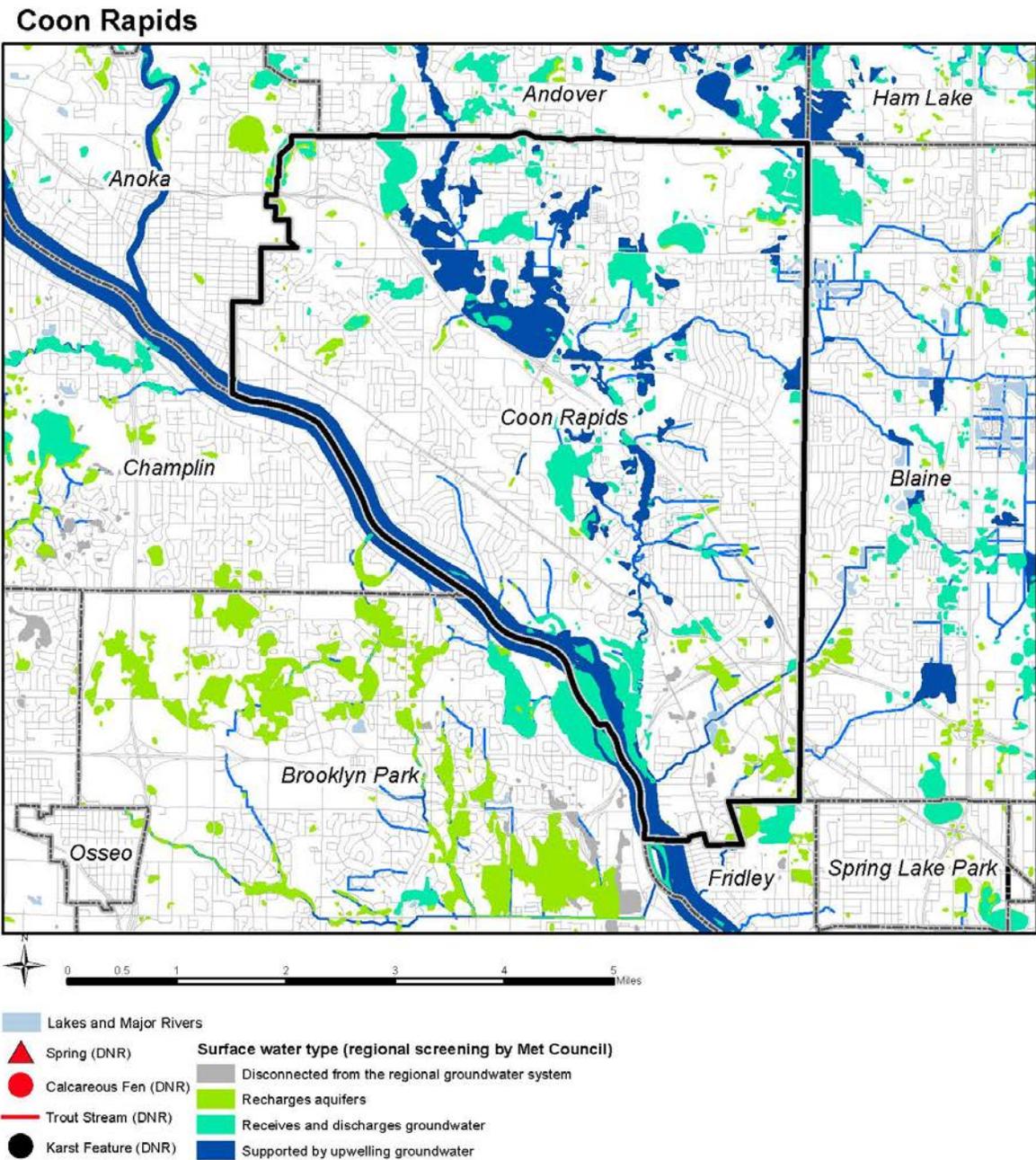
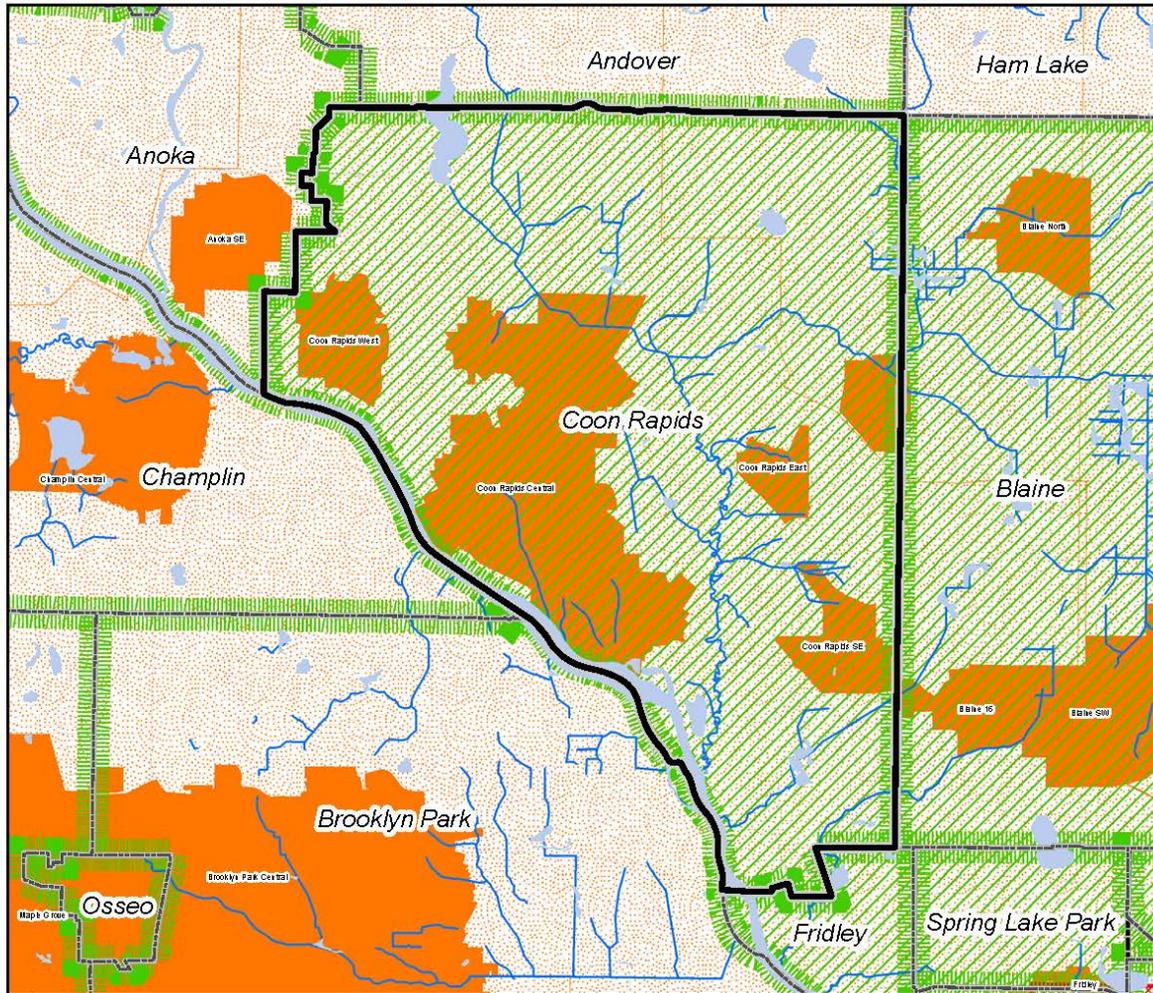




Figure 5. Municipal public water supply system interconnections and regulatory management areas

### Coon Rapids



-  Lakes and Major Rivers
-  The community's most recent local water supply plan reports that the public water supply system has no interconnections
-  The community's most recent local water supply plan reports that the public water supply system has one or more interconnections
-  Special Well and Boring Construction Area (MDH)
-  North and East Metro Groundwater Management Area (DNR)
-  Moderate to Highly Vulnerable Drinking Water Supply Management Area (MDH)
-  Drinking Water Supply Management Area for Minneapolis/St. Paul

# REGIONAL PARKS SYSTEM STATEMENT

*City of Coon Rapids*

The Regional Parks System includes 62 regional parks, park reserves, and special recreation features, plus more than 340 miles of regional trails that showcase the unique landscapes of the region and provide year-round recreation. The Regional Parks System is well-loved by our region's residents and attracted over 48 million annual visits in 2014.

The organizational structure of the Regional Parks System is unique, built upon a strong partnership between the Council and the ten regional park implementing agencies that own and operate Regional Parks System units. The regional park implementing agencies are:

Anoka County	Ramsey County
City of Bloomington	City of Saint Paul
Carver County	Scott County
Dakota County	Three Rivers Park District
Minneapolis Park and Recreation Board	Washington County

The *2040 Regional Parks Policy Plan* was developed based on furthering the *Thrive MSP 2040* outcomes of Stewardship, Prosperity, Equity, Livability, and Sustainability. *Thrive MSP 2040* states that the Council will collaborate with the Metropolitan Parks and Open Space Commission, the regional park agencies, and state partners to:

- Expand the Regional Parks System to conserve, maintain, and connect natural resources identified as being of high quality or having regional importance, as identified in the *2040 Regional Parks Policy Plan*.
- Provide a comprehensive regional park and trail system that preserves high-quality natural resources, increases climate resiliency, fosters healthy outcomes, connects communities, and enhances quality of life in the region.
- Promote expanded multimodal access to regional parks, regional trails, and the transit network, where appropriate.
- Strengthen equitable usage of regional parks and trails by all our region's residents, such as across age, race, ethnicity, income, national origin, and ability.

## **Key Concepts in the 2040 Regional Parks Policy Plan**

The *2040 Regional Parks Policy Plan* includes the following policies, each with specific associated strategies:

- **Recreation Activities and Facilities Policy:** Provide a regional system of recreation opportunities for all residents, while maintaining the integrity of the natural resource base within the Regional Parks System.

- **Siting and Acquisition Policy:** Identify lands with high-quality natural resources that are desirable for Regional Parks System activities and put these lands in a protected status so they will be available for recreational uses and conservation purposes in perpetuity.
- **Planning Policy:** Promote master planning and help provide integrated resource planning across jurisdictions.
- **Finance Policy:** Provide adequate and equitable funding for the Regional Parks System units and facilities in a manner that provides the greatest possible benefits to the people of the region.
- **System Protection Policy:** Protect public investment in acquisition and development by assuring that every component in the system is able to fully carry out its designated role as long as a need for it can be demonstrated.

The *2040 Regional Parks Policy Plan* is the metropolitan system plan for regional recreation open space with which local comprehensive plans must conform. This system statement highlights the elements of the system plan which apply specifically to your community. Find the complete text of the *2040 Regional Parks Policy Plan* on [the Council's website](#).

## ***2040 Regional Parks System Facilities***

The Regional Parks System is comprised of four main types of facilities: regional parks, park reserves, special recreation features and regional trails.

### **Regional Parks**

Regional parks most notably contain a diversity of nature-based resources, either naturally occurring or human-built, and are typically 200-500 acres in size. Regional parks accommodate a variety of passive recreation activities.

### **Park Reserves**

Park reserves, like regional parks, provide for a diversity of outdoor recreation activities. One major feature that distinguishes a park reserve from a regional park is its size. The minimum size for a park reserve is 1,000 acres. An additional characteristic of park reserves is that up to 20 percent of the park reserve can be developed for recreational use, with at least 80 percent of the park reserve to be managed as natural lands that protect the ecological functions of the native landscape.

### **Special Recreation Features**

Special recreation features are defined as Regional Parks System opportunities not generally found in the regional parks, park reserves or trail corridors. Special recreation features often require a unique managing or programming effort.

### **Regional Trails**

Regional trails are classified as 1) destination or greenway trails and 2) linking trails. Destination or greenway trails typically follow along routes with high-quality natural resources that make the trail itself a destination. Linking trails are predominately intended to provide connections between various Regional Parks System facilities, most notably regional parks or park reserves.

## *2040 Regional Parks System Components*

The *2040 Regional Parks Policy Plan* identifies six components which together comprise the vision for the Regional Parks System in 2040, as described below.

**Existing Regional Parks System Facilities:** include Regional Parks System Facilities that are open for public use. These facilities include land that is owned by regional park implementing agencies, and may include inholding parcels within the boundaries of these parks and trail corridors that have not yet been acquired. Existing regional trails may include planned segments that will be developed in the future.

**Planned Regional Parks System Facilities (not yet open to the public):** include Regional Parks System Facilities that have a Council-approved master plan and may be in stages of acquisition and development, but are not yet open for public use.

**Regional Parks System Boundary Adjustments:** include general areas identified as potential additions to existing Regional Parks System Facilities to add recreational opportunities or protect natural resources. Specific adjustments to park or trail corridor boundaries have not yet been planned.

**Regional Park Search Areas:** include general areas for future regional parks to meet the recreational needs of the region by 2040 where the regional park boundary has not yet been planned.

**Regional Trail Search Corridors:** include proposed regional trails to provide connections between Regional Parks System facilities where the trail alignment has not yet been planned.

**2040 Regional Trail Search Corridor System Additions:** include regional trail search corridors that were added to the Regional Parks System as part of the *2040 Regional Parks Policy Plan*.

## *Key Changes in the 2040 Regional Parks Policy Plan*

Adopted by the Metropolitan Council in February 2015, the *2040 Regional Parks Policy Plan* incorporates the following changes:

### **Identify all proposed regional trails as regional trail search corridors**

All proposed regional trails that are not yet open to the public and do not have a Metropolitan Council approved master plan are represented as a general regional trail search corridor. The *2030 Regional Parks Policy Plan* depicted these trails with a proposed alignment. The alignment of these regional trails will be determined in the future through a planning process led by the regional park implementing agency. The alignment of these trails is subject to Metropolitan Council approval of a regional trail master plan.

### **Acquire and develop ten new regional trails or trail extensions to meet the needs of the region in 2040. The 2040 Regional Trail Search Corridor Additions include:**

#### Carver County:

- County Road 61
- Highway 41

### Three Rivers Park District:

- CP Rail Extension
- Dakota Rail Extension
- Lake Independence Extension
- Lake Sarah Extension
- Minnetrista Extension
- North-South 1
- North-South 2
- West Mississippi River

The *2040 Regional Parks System Plan Map* is depicted in Figure 1. Coon Rapids should consult the complete [2040 Regional Parks Policy Plan](#) in preparing its local comprehensive plan. In addition, Coon Rapids should consult *Thrive MSP 2040* and the current version of the Metropolitan Council's *Local Planning Handbook* for specific information needed in its comprehensive plan.

## **System Plan Considerations Affecting Your Community**

### *Regional Parks System Components in your community*

The following Regional Parks System Components within Coon Rapids as identified in the *2040 Regional Parks Policy Plan* are listed below.

#### *Regional Parks, Park Reserves, and Special Recreation Features*

**Bunker Hills Regional Park:** This is an existing regional park with an established boundary. The regional park boundary as shown in Figure 2 should be acknowledged in the comprehensive plan.

**Coon Rapids Dam Regional Park:** This is an existing regional park with an established boundary. The regional park boundary as shown in Figure 2 should be acknowledged in the comprehensive plan.

#### *Regional Trails*

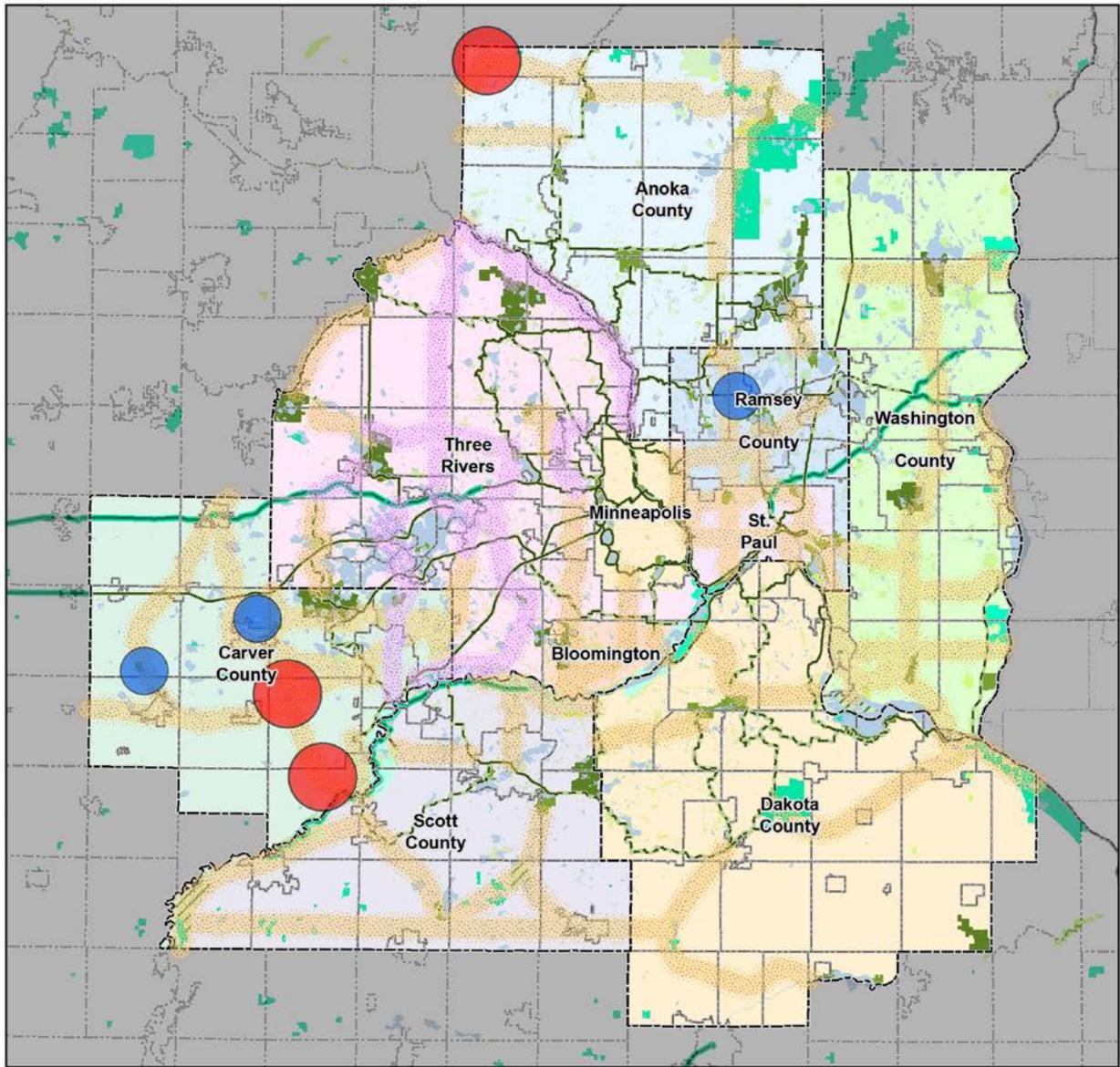
**Coon Creek Regional Trail:** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through Coon Rapids as it connects Coon Rapids Dam Regional Park and Bunker Hills Regional Park. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.

**Mississippi River Regional Trail (Anoka County):** This is a regional trail that includes segments that are open to the public as well as planned segments that will be developed in the future. The regional trail travels through Ramsey, Anoka, Coon Rapids, and Fridley as it connects Mississippi West Regional Park, Rum River Regional Trail, Coon Rapids Dam Regional Park, Rice Creek West Regional Trail, and Anoka County Riverfront Regional Park. The regional trail alignment as shown in Figure 2 should be acknowledged in the comprehensive plan.

Please contact Anoka County for more information regarding Regional Parks System Components in Coon Rapids.

Figure 1. 2040 Regional Parks System Plan Map

## Regional Parks System Twin Cities Metropolitan Area



### Regional Parks

- Regional Parks
- Park Reserves
- Special Recreation Features
- Planned Units
- Regional Trail Corridor Land

### Regional Trails

- Existing
- Planned

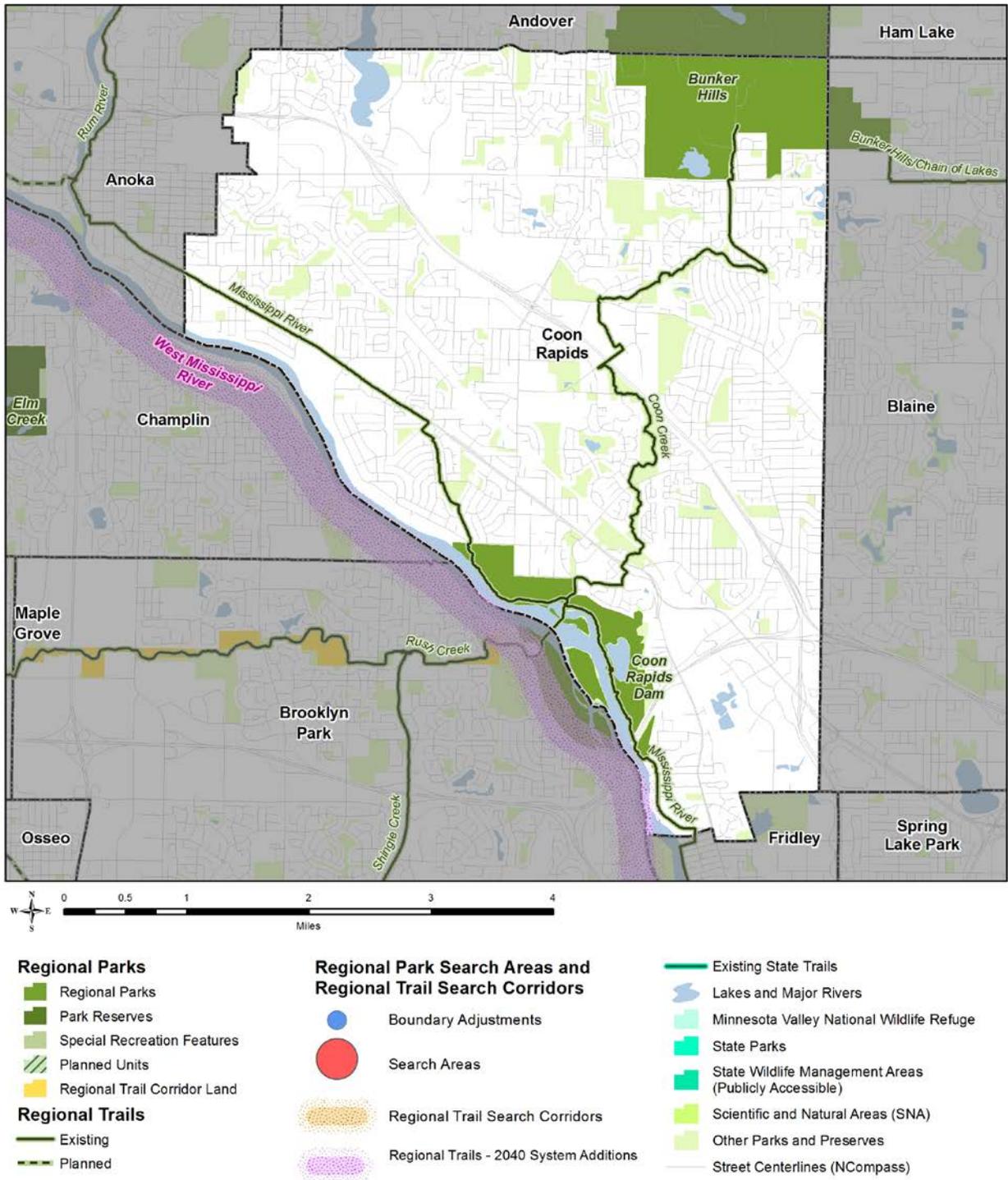
### Regional Park Search Areas and Regional Trail Search Corridors

- Boundary Adjustments
- Search Areas
- Regional Trail Search Corridors
- Regional Trails - 2040 System Additions

- Existing State Trails
- Lakes and Major Rivers
- Minnesota Valley National Wildlife Refuge
- State Parks
- State Wildlife Management Areas (Publicly Accessible)
- Scientific and Natural Areas (SNA)
- Other Parks and Preserves

Figure 2. Regional Parks System Facilities in and adjacent to Coon Rapids

## Regional Parks System City of Coon Rapids, Anoka County



**2018 Comprehensive Plan Update  
Key Dates and Timelines**

April 2016	Staff level discussions on Comp Plan update options and process. Define roles and responsibilities. Create interdepartmental team to assist with appropriate sections of document.
May 2016	City Council discussion and direction on process. Discuss role for commissions, public and other stakeholders.
June 2016	Meet with potential consultant/s to assist with appropriate elements. Develop communication and outreach plan.
Summer 2016	Begin discussions with various city commissions on current Comprehensive Plan, new elements and process for updating 2018 plan. Describe roles and responsibilities. Staff/consultant to begin updating technical components, data gathering, etc.
Fall 2016	City Commissions to look at plan elements and provide feedback. Begin preparations for public input meetings. Staff to prepare draft of 2018 Comp Plan.
Winter 2017	Update to Council on process. Commissions to review draft 2018 Comp Plan for feedback.
Summer 2017	Public meetings for input on draft 2018 Comp Plan.
Fall 2017	Update to Council on process. City Council to consider approval.
Winter 2018	Distribute draft 2018 Comp Plan to agencies for formal comment (60 days).
Spring 2018	Submit draft 2018 Comp Plan to Met Council. Met Council has 15 days to determine completeness. If accepted, agency has 60 days for administrative review.
Summer 2018	Final adoption by City Council.
<hr/>	
Sept. 2018	Latest Council can adopt plan.
Dec. 2018	Latest City can submit adopted 2018 Comp Plan to Met Council.